

Section 3

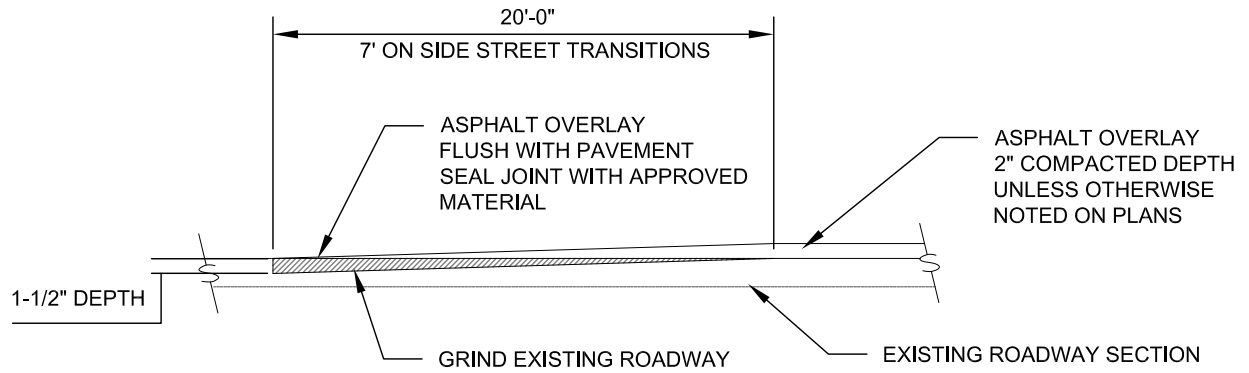
Roadway Construction

	HMA CLASS 1/2" PG 64-22 (IN.)	HMA CLASS 1" PG 64-22 (IN.)	CONCRETE
ARTERIAL	4	5	9
LOCAL ACCESS (PUBLIC)	7	0	6
COMMERCIAL/INDUSTRIAL	4	5	9
PRIVATE STREET/PARKING LOT	3	NOTE 2	6



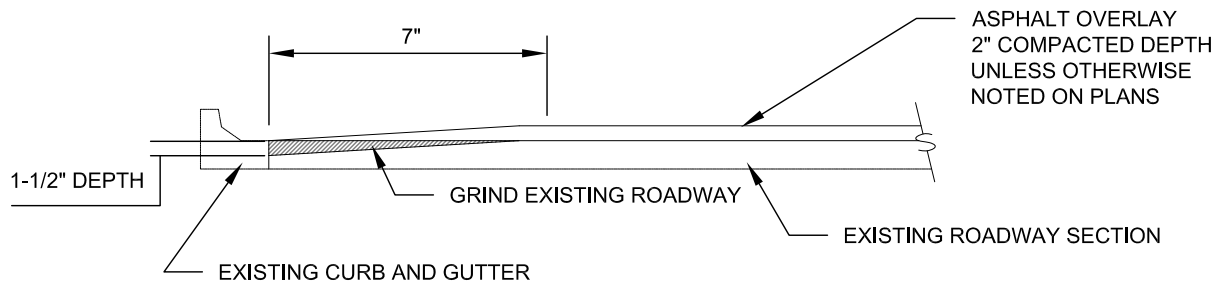
5. FRANCHISE AND DRY UTILITIES SHOULD BE LOCATED IN EASEMENTS OUTSIDE RIGHT-OF-WAY OR AS APPROVED BY CITY ENGINEER.

DETAIL NUMBER: 301



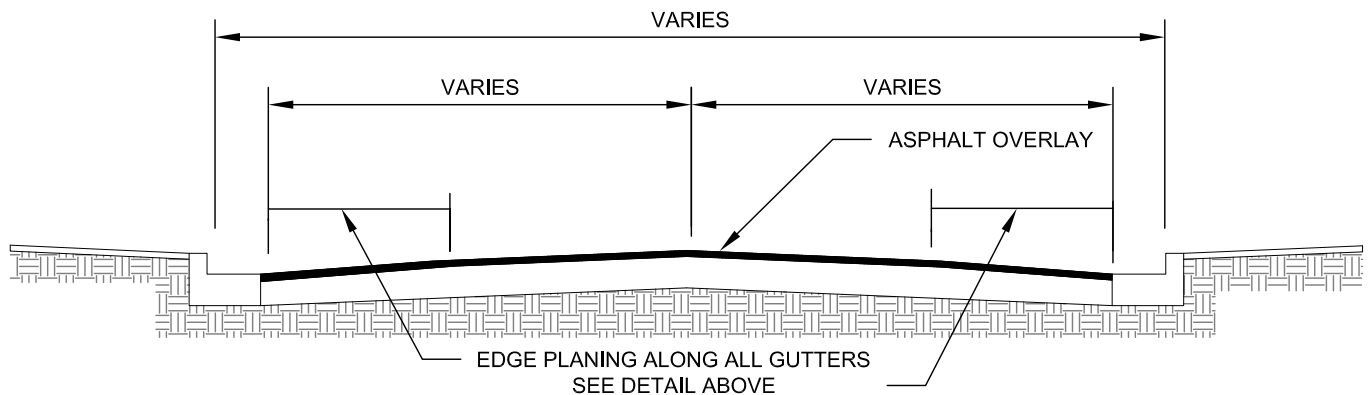
BUTT JOINT DETAIL

NTS



EDGE PLANING DETAIL

NTS



TYPICAL OVERLAY DETAIL

NTS

Rob Crittenden

APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2017

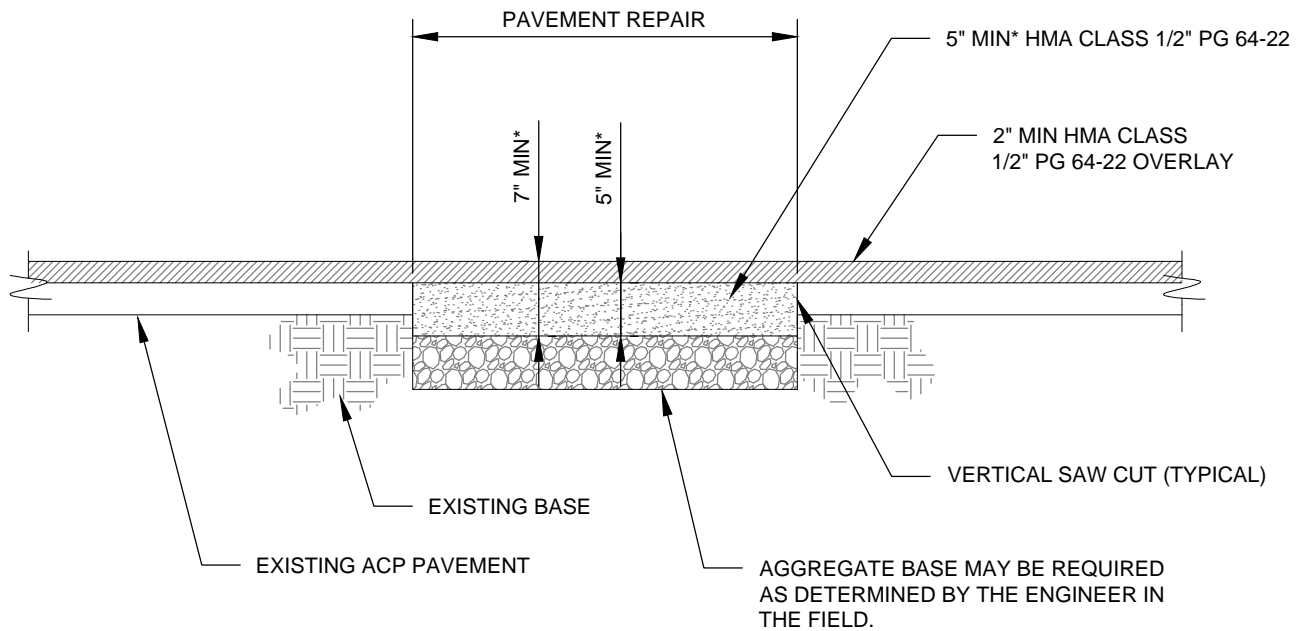


STANDARD DETAILS

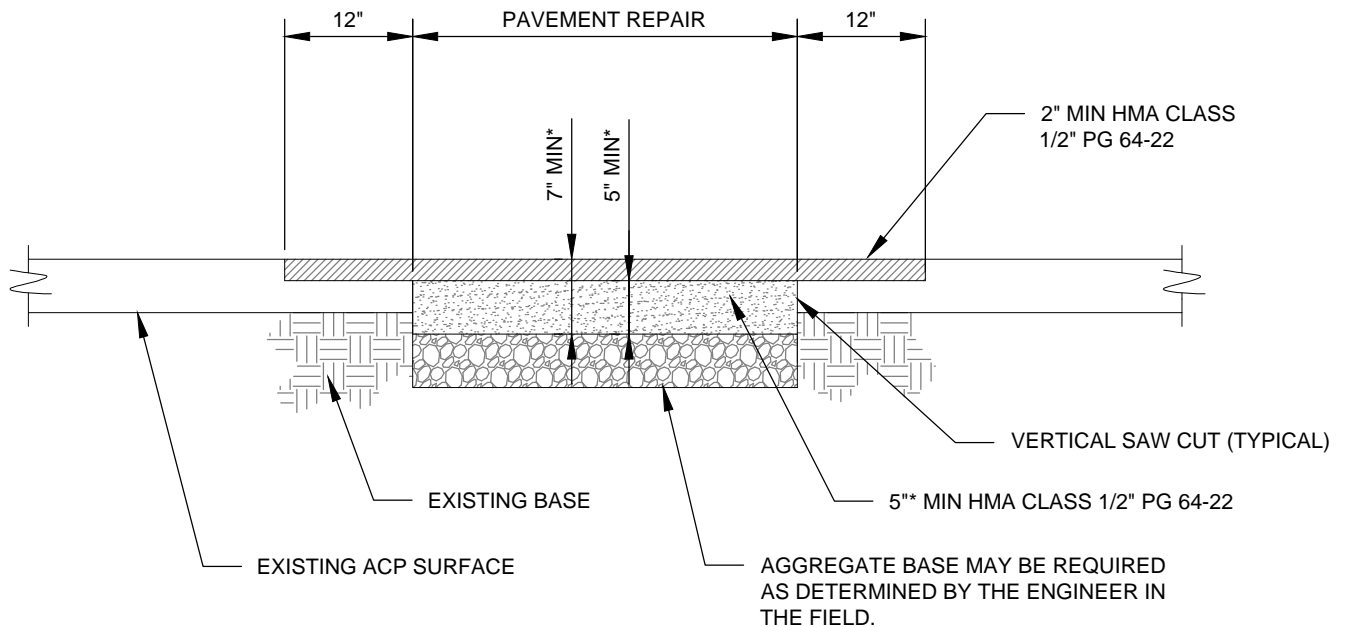
PAVEMENT PLANING DETAILS

FILE NAME: SD302.DWG

DETAIL NUMBER: **302**



TYPICAL PAVEMENT REPAIR DETAIL WITH OVERLAY
NTS



TYPICAL PAVEMENT REPAIR DETAIL
NTS

NOTE:

* SEE TYPICAL ROADWAY SECTIONS STANDARD DETAIL #301 FOR PAVEMENT DEPTHS.

Rob Crittenden

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TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2017



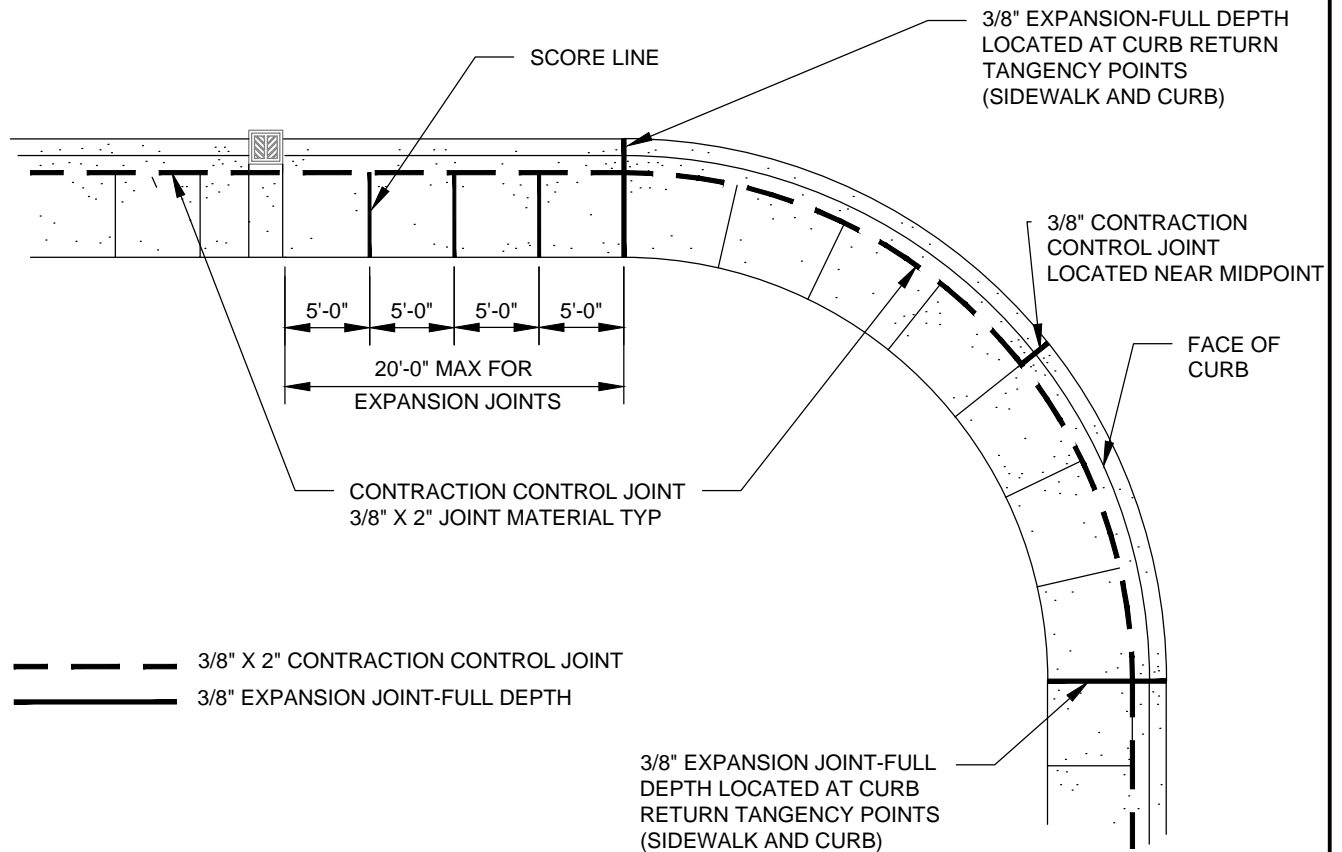
City of Redmond
WASHINGTON

STANDARD DETAILS

PAVEMENT REPAIR DETAILS

FILE NAME: SD302A.DWG

DETAIL NUMBER: 302A



PLAN VIEW

NTS

NOTES:

1. SCORE LINE SHALL BE 1/4 INCH DEEP V-GROVE.
2. JOINTS SHALL MATCH THE CURB.
3. SIDEWALK SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 8-14 OF THE STANDARD SPECIFICATIONS AND AS SHOWN ON THE STANDARD DETAILS
4. SIDEWALK SHALL BE 6 INCH MINIMUM THICKNESS WHERE ADJACENT TO ROLLED CURB SECTION, OTHERWISE MINIMUM THICKNESS SHALL BE 4 INCH.
5. THE CONCRETE MIX FOR SIDEWALKS SHALL BE AIR ENTRAINED CONCRETE CLASS 3000 IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 6-02.
6. PLACING AND FINISHING OF SIDEWALKS SHALL BE PER SECTION 8-14 OF THE STANDARD SPECIFICATION. THE SURFACES ARE TO BE STRUCK OFF, TROWELED, LIGHTLY BRUSHED IN TRANSVERSE DIRECTION, THEN JOINED AND EDGED. THE FINISH REQUIREMENTS INCLUDE:
 - A. JOINTS SHALL BE TOOLED WITH 1/4 INCH RADIUS EDGER.
 - B. SIDEWALK EDGES TOOLED WITH A 1/2 INCH EDGER.
 - C. WHEN REPLACING SECTIONS OF EXISTING SIDEWALK OR WHEN NEW SIDEWALK ADJOINS EXISTING, NEW CONCRETE SHALL BE FINISHED TO MATCH THE EXISTING CONCRETE OR AS DIRECTED BY THE CITY ENGINEER. COLORING AGENT SHALL BE USED IN NEW CONCRETE TO MATCH EXISTING.
 - D. WHEN CASTINGS ARE LOCATED IN THE SIDEWALK, JOINTS SHALL BE INSTALLED TO CONTROL CRACKING. REBAR SHALL BE INSTALLED AS DIRECTED BY THE CITY TO STRENGTHEN SIDEWALK TO PREVENT CRACKING AT CORNERS OF CASTINGS AND OTHER BLOCKOUTS.

Donald Cairns

APPROVED BY: DONALD CAIRNS
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REVISION DATE: MARCH 01, 2018

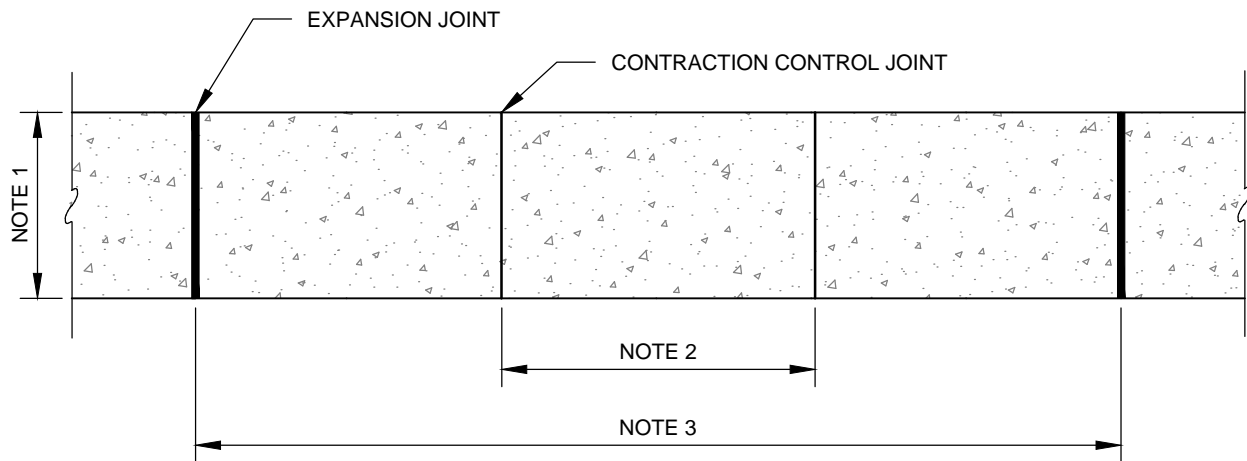


STANDARD DETAILS

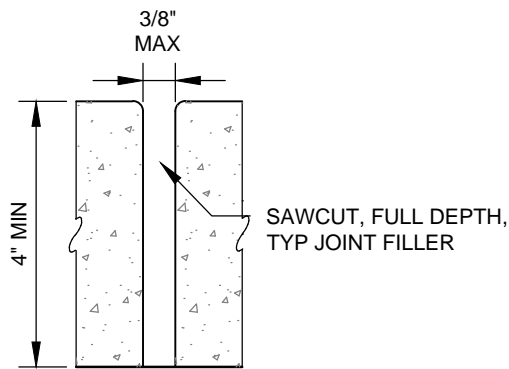
CURB AND SIDEWALK JOINTS

FILE NAME: SD303.DWG

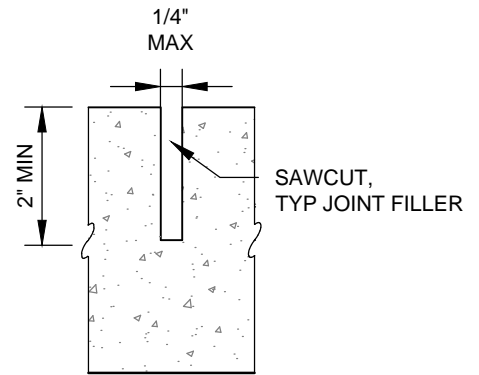
DETAIL NUMBER: 303



PLAN VIEW
NTS



EXPANSION JOINT SECTION
NTS



CONTRACTION CONTROL JOINT SECTION
NTS

NOTES:

1. SHARED USE PATH WIDTH SHALL BE IN ACCORDANCE WITH DESIGN MANUAL M-22-01.09 CHAPTER 1515 AS DIRECTED BY CITY ENGINEER. MINIMUM VERTICAL CLEARANCE OF PATH SHALL BE 12 FEET.
2. CONTRACTION JOINTS SHALL BE AT 5-FOOT INTERVALS OR INTERVALS EQUAL TO SHARED USE PATH WIDTH AS DIRECTED BY CITY ENGINEER.
3. EXPANSION JOINTS SHALL BE INSTALLED AT 200-FOOT MAXIMUM INTERVALS AS DIRECTED BY CITY ENGINEER. EXPANSION JOINTS SHALL ALSO BE INSTALLED AT CURB RAMPS AND FIXED STRUCTURES AS PER STANDARD SPECIFICATIONS SECTION 8-14.
4. LONGITUDINAL JOINTS ARE NOT ALLOWED.
5. IF SHARED USE PATH IS ADJACENT TO CURB, JOINTS SHALL MATCH THE CURB.
6. SHARED USE PATH SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 8-14 OF STANDARD SPECIFICATIONS, EXCEPT THAT ALL JOINTS SHALL BE SAWCUT.

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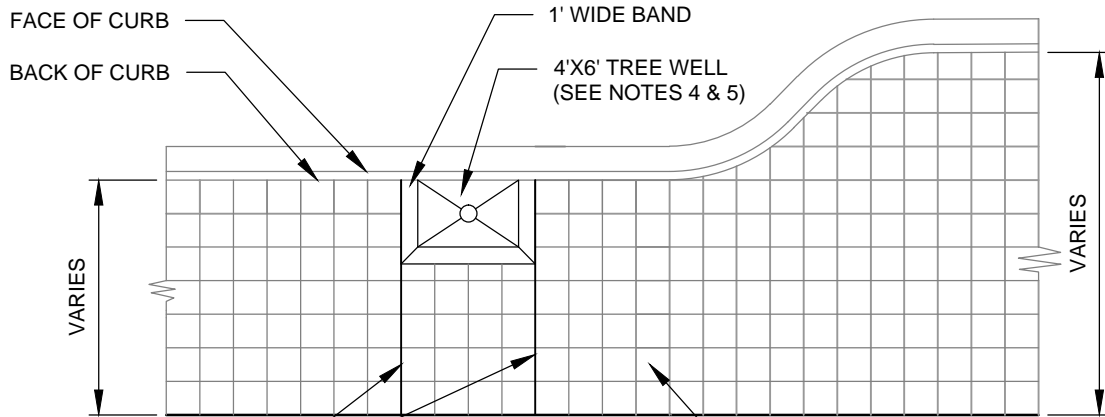
City of Redmond
WASHINGTON

STANDARD DETAILS

CONCRETE SHARED
USE PATH

FILE NAME: SD303A.DWG

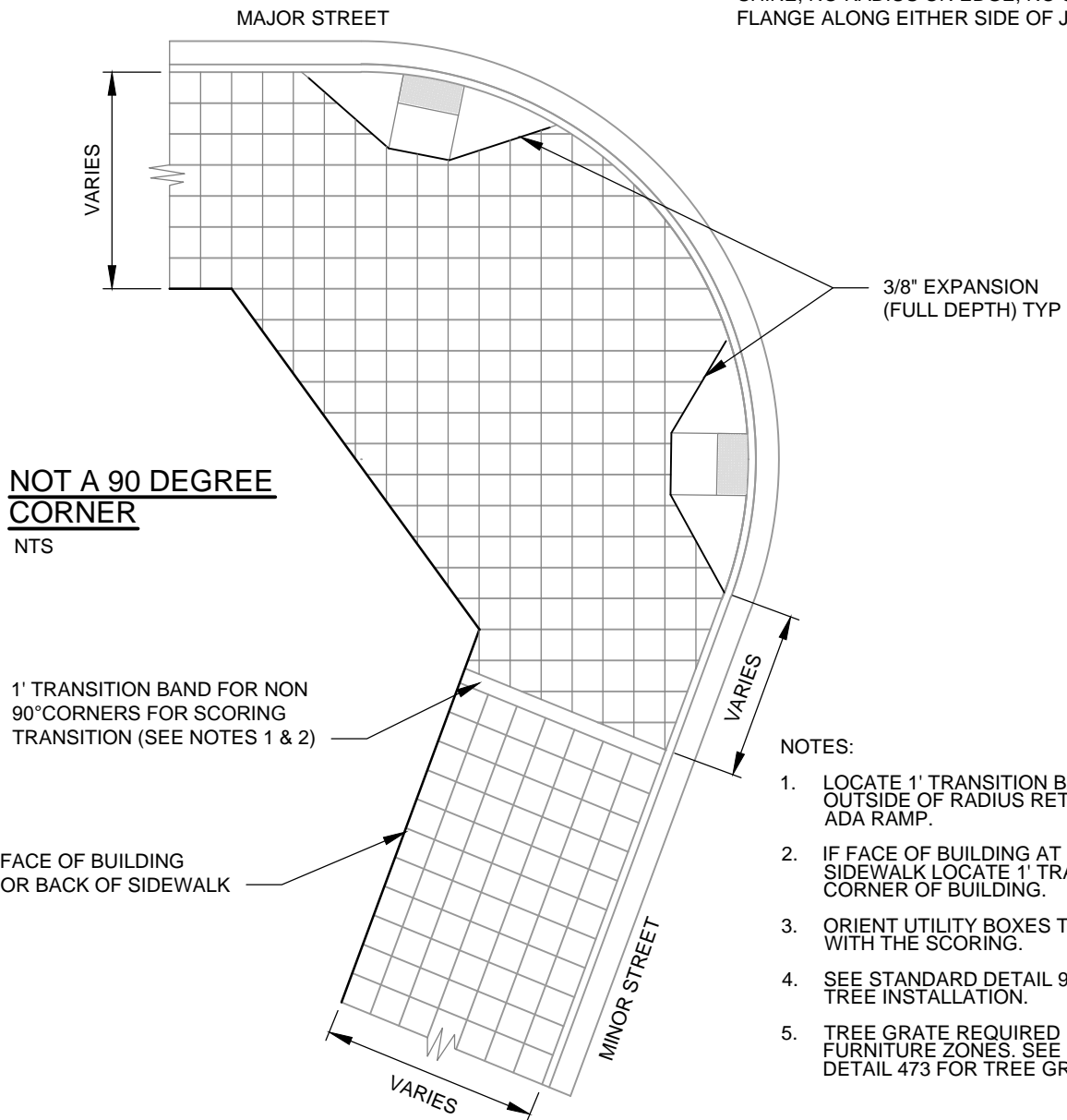
DETAIL NUMBER: 303A



PLAN VIEW
NTS

3/8" EXPANSION
(FULL DEPTH) TYP

2' X 2' SCORED CONCRETE. SCORE TO BE
SAWCUT AT 1/2" DEPTH X 1/8" WIDE. NO
SHINE, NO RADIUS ON EDGE, NO SMOOTH
FLANGE ALONG EITHER SIDE OF JOINT.



**NOT A 90 DEGREE
CORNER**
NTS

1' TRANSITION BAND FOR NON
90° CORNERS FOR SCORING
TRANSITION (SEE NOTES 1 & 2)

FACE OF BUILDING
OR BACK OF SIDEWALK

NOTES:

1. LOCATE 1' TRANSITION BAND
OUTSIDE OF RADIUS RETURN AND
ADA RAMP.
2. IF FACE OF BUILDING AT BACK OF
SIDEWALK LOCATE 1' TRANSITION AT
CORNER OF BUILDING.
3. ORIENT UTILITY BOXES TO ALIGN
WITH THE SCORING.
4. SEE STANDARD DETAIL 907 FOR
TREE INSTALLATION.
5. TREE GRATE REQUIRED IN
FURNITURE ZONES. SEE STANDARD
DETAIL 473 FOR TREE GRATE DETAIL.

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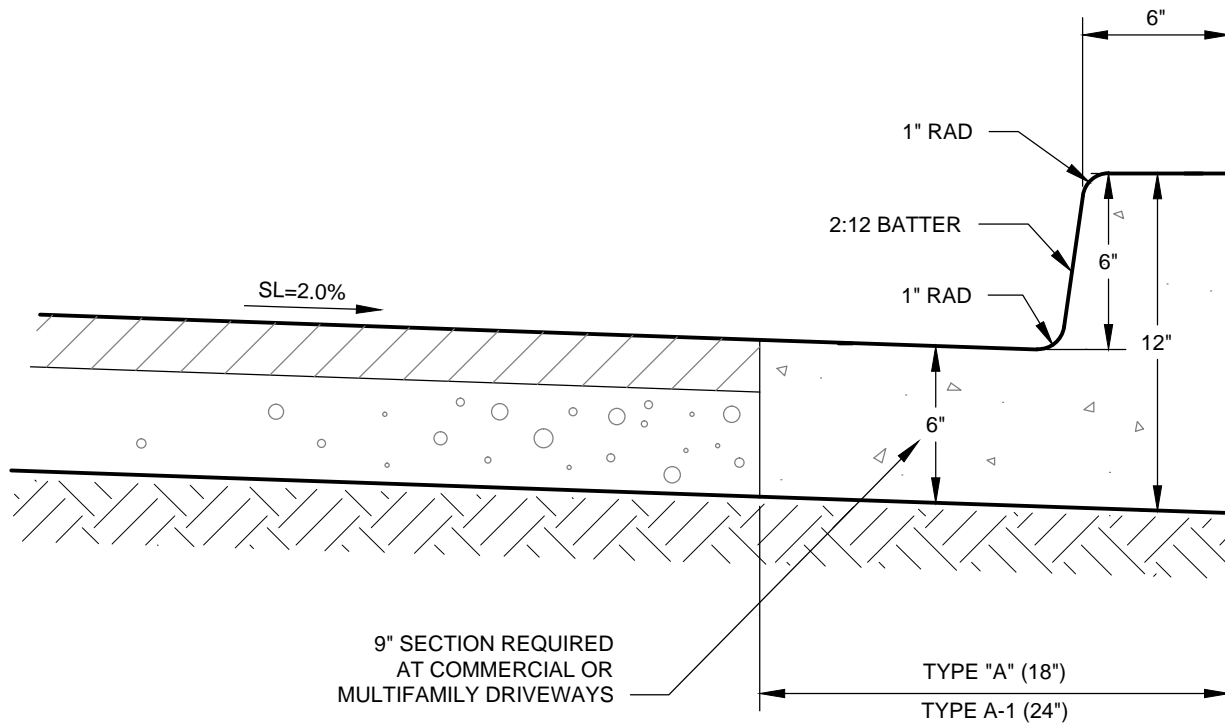
REVISION DATE: MARCH 01, 2018



STANDARD DETAILS

**DOWNTOWN AND OVERLAKE
PEDESTRIAN SIDEWALK**

FILE NAME: SD303B.DWG DETAIL NUMBER: 303B



TYPE A AND A-1 CURB AND GUTTER SECTION

NTS

NOTES:

1. CONCRETE SHALL BE AIR ENTRAINED CLASS 3000 PER WSDOT STANDARD SPECIFICATIONS 6-02 CLASS 4000 REQUIRED ALONG DRIVEWAY ENTRANCE.
2. TYPE "A" AND ROLLED CURB AND GUTTER SECTIONS MAY BE PERMITTED FOR NEW PRIVATE STREETS.
3. TYPE "A" OR ROLLED CURB AND GUTTER MAY BE PERMITTED ON PUBLIC STREETS ONLY TO MATCH EXISTING.
4. CURB AND GUTTER SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 8-04 OF THE STANDARD SPECIFICATIONS.
5. AFTER THE CONCRETE HAS SET SUFFICIENTLY, THE ROADWAY FACE OF THE CURB FORMS SHALL BE REMOVED AND THE TOP AND FACE OF THE CURB SHALL RECEIVE A LIGHT BRUSH FINISH, AND THE TOP OF THE GUTTER SHALL RECEIVE A BROOM FINISH.

Rob Crittenden

APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2018



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STANDARD DETAILS

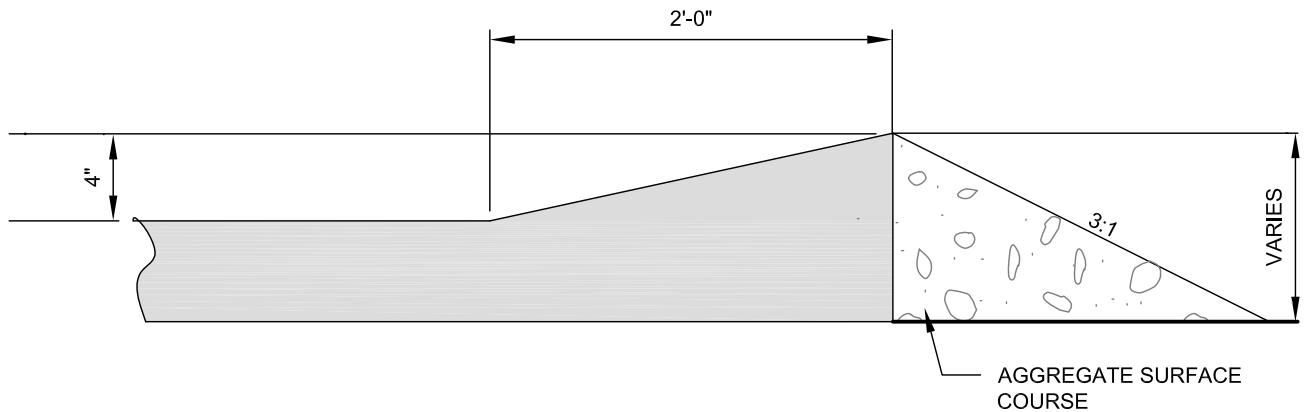
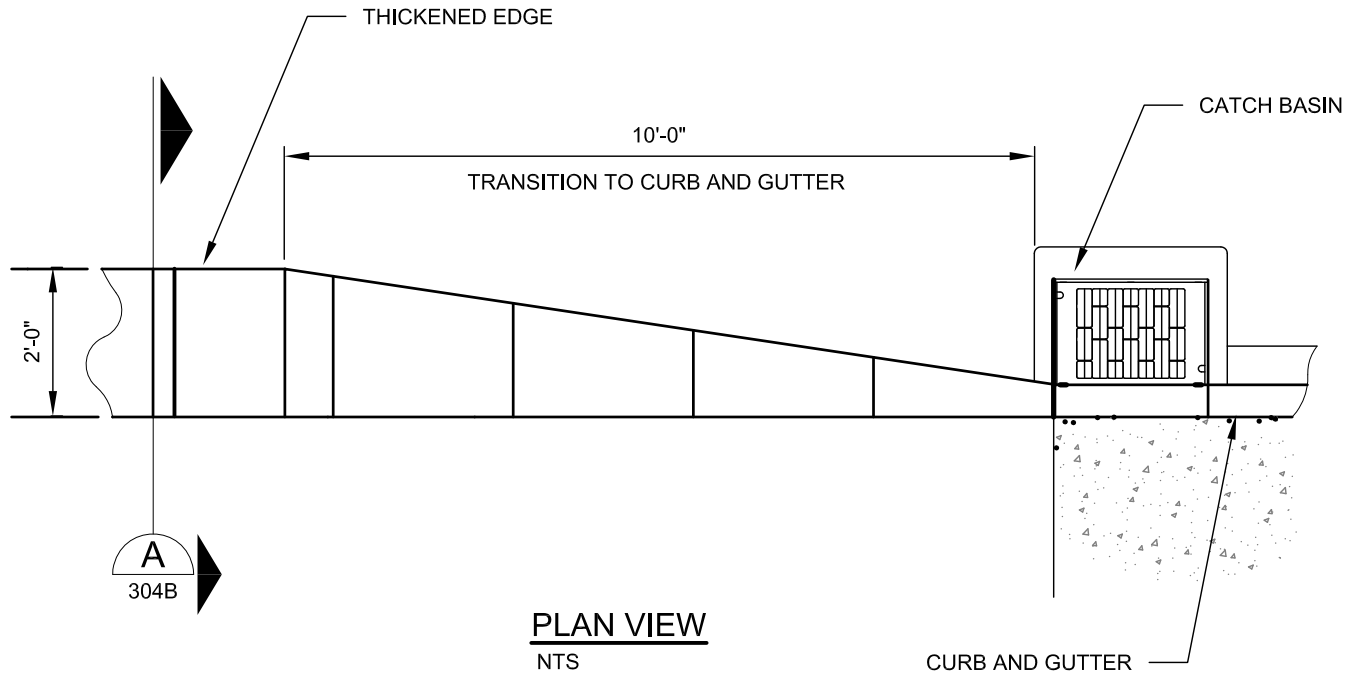
**TYPICAL CURB AND GUTTER
DETAILS**

FILE NAME: SD304.DWG

DETAIL NUMBER: 304



FILE NAME: SD304A.DWG	DETAIL NUMBER: 304A
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Rob Crittenden

APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2014

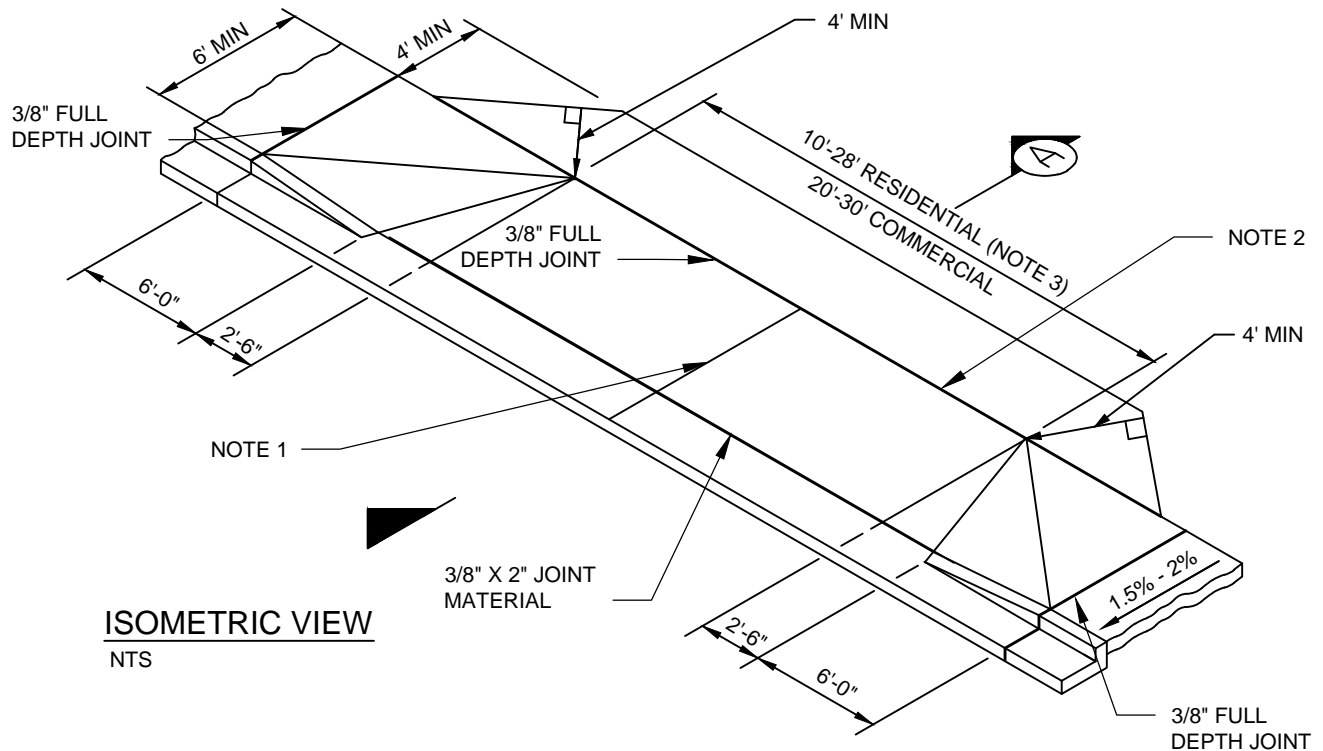


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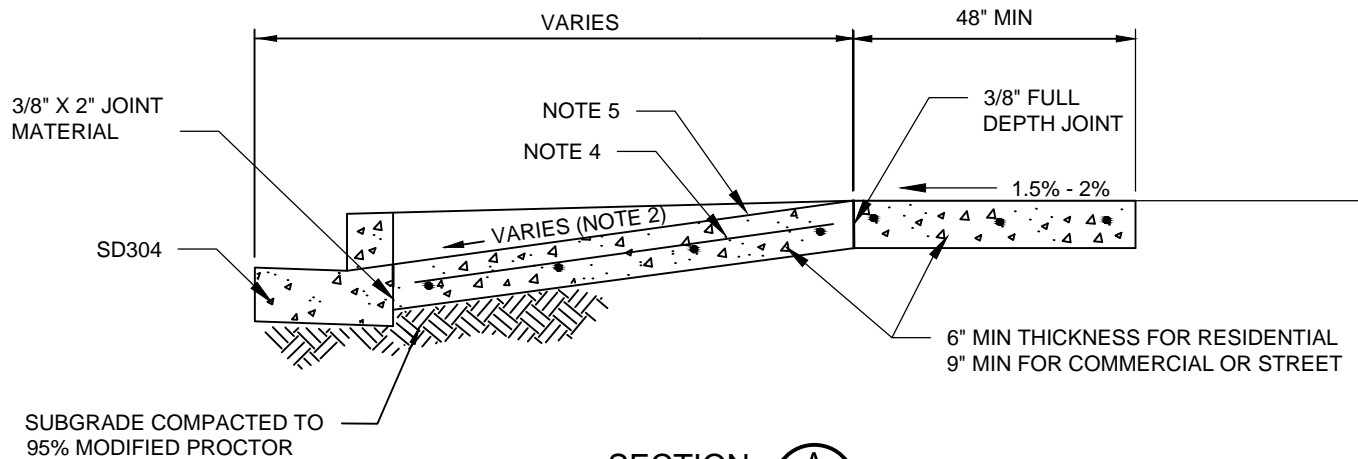
STANDARD DETAILS

ASPHALT THICKENED EDGE

FILE NAME: SD304B.DWG | DETAIL NUMBER: **304B**



ISOMETRIC VIEW
NTS



SECTION A
NTS

NOTES:

1. 3/8 INCH X 2 INCH JOINT MATERIAL ON CENTERLINE WHEN DRIVEWAY IS WIDER THAN 16 FEET.
2. IF DRIVEWAY SLOPE EXCEEDS 2%, A 48 INCH WALKWAY IS REQUIRED BEHIND THE DRIVEWAY AND FLARE.
3. SEE RZC APPENDIX 2 SECTION D.3 AND TABLE 11 FOR DETAILS.
4. DRIVEWAYS SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 5-05 AND 8-06 OF THE STANDARD SPECIFICATIONS. WIRE MESH SHALL BE INSTALLED AT MID-SECTION ON COMMERCIAL DRIVEWAYS. WIRE MESH SHALL BE 4 INCH X 4 INCH, NUMBER 4 GAUGE PER SECTION 9-07.7 OF THE STANDARD SPECIFICATIONS (AASHTO M55).
5. THE CONCRETE MIX FOR DRIVEWAYS SHALL BE AIR ENTRAINED CONCRETE CLASS 4000 IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 6-02 OF THE STANDARD SPECIFICATIONS. THE CONCRETE FINISH REQUIREMENTS INCLUDE:
 - A. JOINTS SHALL BE TOOLED WITH 1/4 INCH RADIUS EDGER
 - B. DRIVEWAY EDGES TOOLED WITH A 1/2 INCH RADIUS EDGER
 - C. AFTER DRIVEWAY IS GIVEN A LONGITUDINAL (TRANSVERSE TO VEHICLE FLOW) BRUSH FINISH, THE EDGES OF THE DRIVEWAY AND ALL JOINTS SHALL BE LIGHTLY EDGED AGAIN WITH A 4 INCH WIDE EDGING TOOL TO GIVE A FINISHED APPEARANCE.

Donald Cairns

APPROVED BY: DONALD CAIRNS
TRANSPORTATION PLANNING AND ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2018

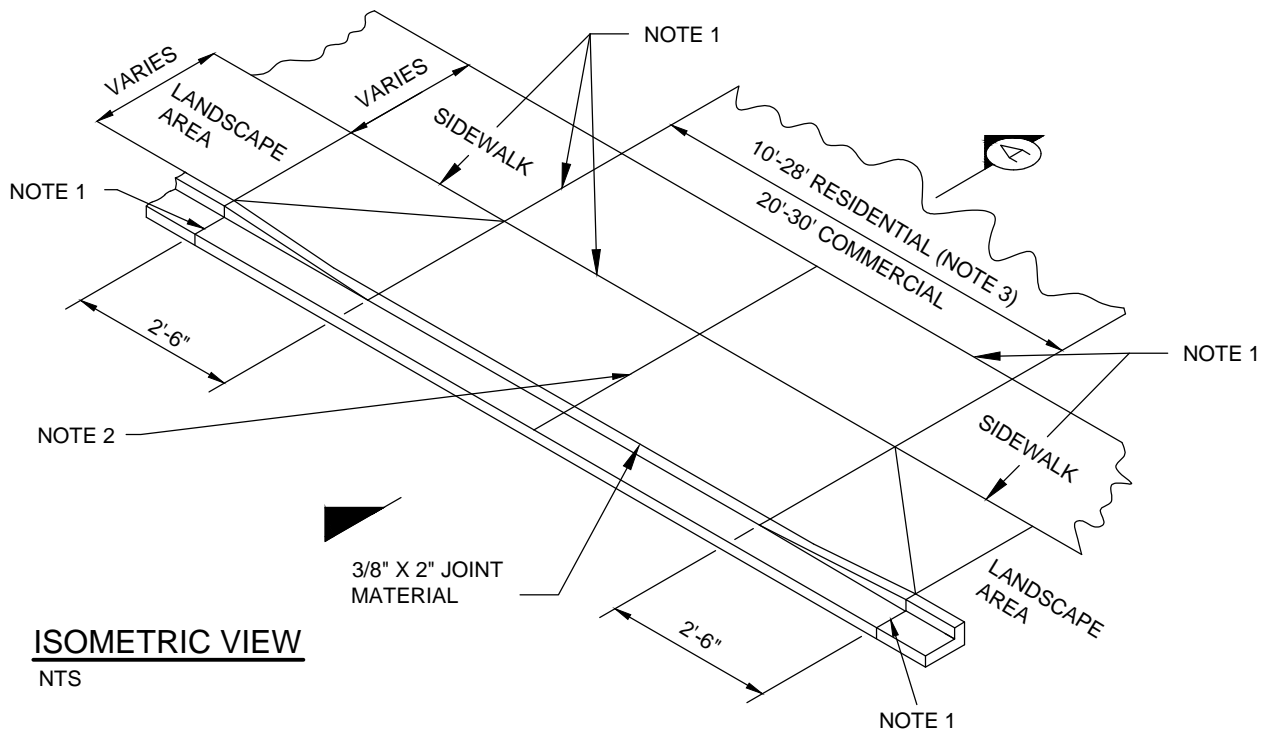


STANDARD DETAILS

TYPE "1" DRIVEWAY

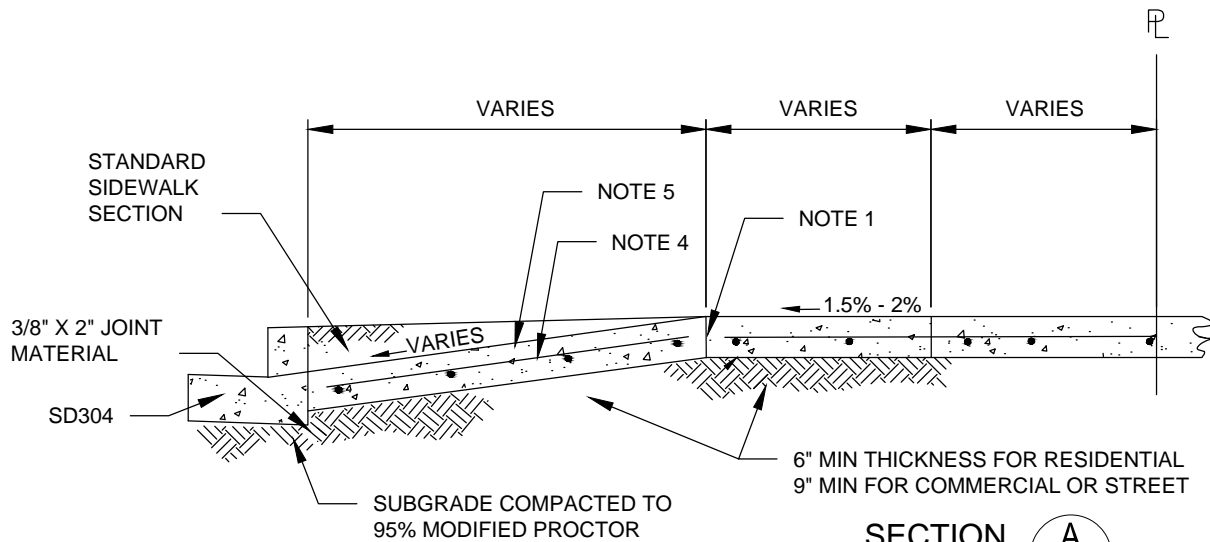
FILE NAME: SD305.DWG

DETAIL NUMBER: 305



ISOMETRIC VIEW

NTS



SECTION A

NTS

NOTES:

1. 3/8 INCH FULL DEPTH JOINT.
2. 3/8 INCH X 2 INCH JOINT MATERIAL ON CENTERLINE WHEN DRIVEWAY IS WIDER THAN 16 FEET.
3. SEE RZC APPENDIX 2 SECTION D.3 AND TABLE 11 FOR DETAILS.
4. DRIVEWAYS SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 5-05 AND 8-06 OF THE STANDARD SPECIFICATIONS. WIRE MESH SHALL BE INSTALLED ON COMMERCIAL DRIVEWAYS. WIRE MESH SHALL BE 4 INCH X 4 INCH, NUMBER 4 GAUGE PER SECTION 9-07 OF THE STANDARD SPECIFICATIONS (AASHTO M55).
5. THE CONCRETE MIX FOR DRIVEWAYS SHALL BE AIR ENTRAINED CONCRETE CLASS 4000 IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 6-02 OF THE STANDARD SPECIFICATIONS. THE CONCRETE FINISH REQUIREMENTS INCLUDE:
 - A. JOINTS SHALL BE TOOLED WITH 1/4 INCH RADIUS EDGER
 - B. DRIVEWAY EDGES TOOLED WITH A 1/2 INCH RADIUS EDGER
 - C. AFTER DRIVEWAY IS GIVEN A LONGITUDINAL (TRANSVERSE TO VEHICLE FLOW) BRUSH FINISH, THE EDGES OF THE DRIVEWAY AND ALL JOINTS SHALL BE LIGHTLY EDGED AGAIN WITH A 4 INCH WIDE EDGING TOOL TO GIVE A FINISHED APPEARANCE.

Donald Cairns

APPROVED BY: DONALD CAIRNS
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REVISION DATE: MARCH 01, 2018

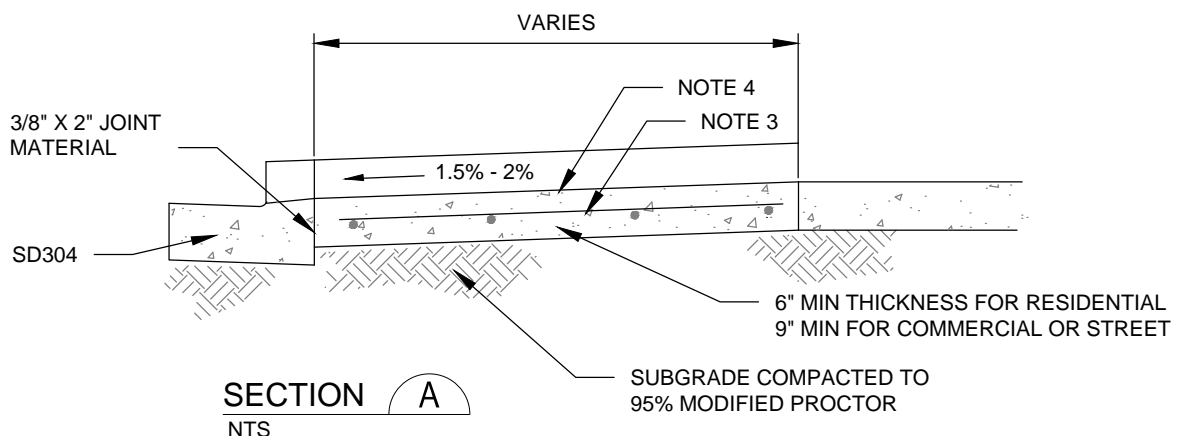
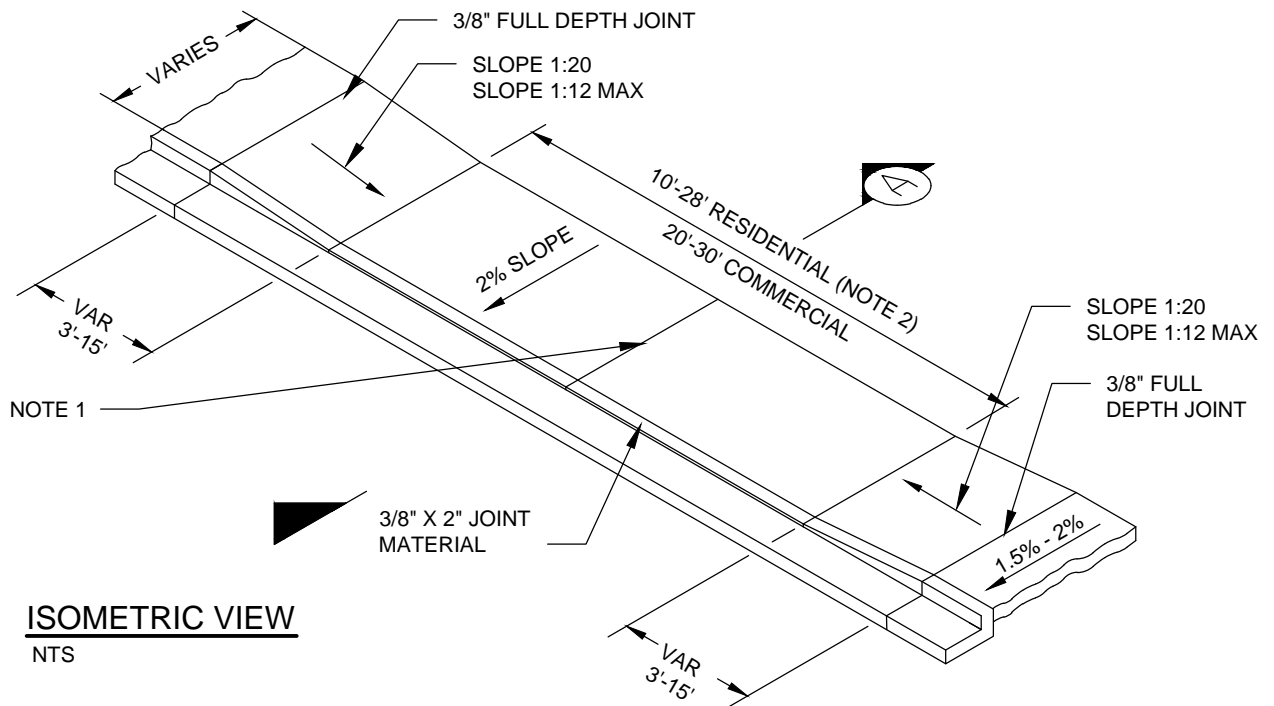


City of Redmond
WASHINGTON

STANDARD DETAILS

TYPE "1A" DRIVEWAY
W/ ADJACENT LANDSCAPE AREA

FILE NAME: SD305A.DWG DETAIL NUMBER: 305A



NOTES:

1. 3/8 INCH X 2 INCH JOINT MATERIAL ON CENTERLINE WHEN DRIVEWAY IS WIDER THAN 16 FEET.
2. SEE RZC APPENDIX 2 SECTION D.3 AND TABLE 11 FOR DETAILS.
3. DRIVEWAYS SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 5-05 AND 8-06 OF THE STANDARD SPECIFICATIONS. WIRE MESH SHALL BE INSTALLED ON COMMERCIAL DRIVEWAYS. WIRE MESH SHALL BE 4 INCH X 4 INCH, NUMBER 4 GAUGE PER SECTION 9-07.7 OF THE STANDARD SPECIFICATIONS (AASHTO M55).
4. THE CONCRETE MIX FOR DRIVEWAYS SHALL BE AIR ENTRAINED CONCRETE CLASS 4000 IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 6-02 OF THE STANDARD SPECIFICATIONS. THE CONCRETE FINISH REQUIREMENTS INCLUDE:
 - A. JOINTS SHALL BE TOOLED WITH 1/4 INCH RADIUS EDGER
 - B. DRIVEWAY EDGES TOOLED WITH A 1/2 INCH RADIUS EDGER
 - C. AFTER DRIVEWAY IS GIVEN A LONGITUDINAL (TRANSVERSE TO VEHICLE FLOW) BRUSH FINISH, THE EDGES OF THE DRIVEWAY AND ALL JOINTS SHALL BE LIGHTLY EDGED AGAIN WITH A 4 INCH WIDE EDGING TOOL TO GIVE A FINISHED APPEARANCE.
5. TYPE "2" DRIVEWAY TO BE USED WHERE ADJOINING PROPERTY IS LOWER THAN SIDEWALK GRADE AND SHALL BE APPROVED BY THE CITY.

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TRANSPORTATION PLANNING AND ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2018



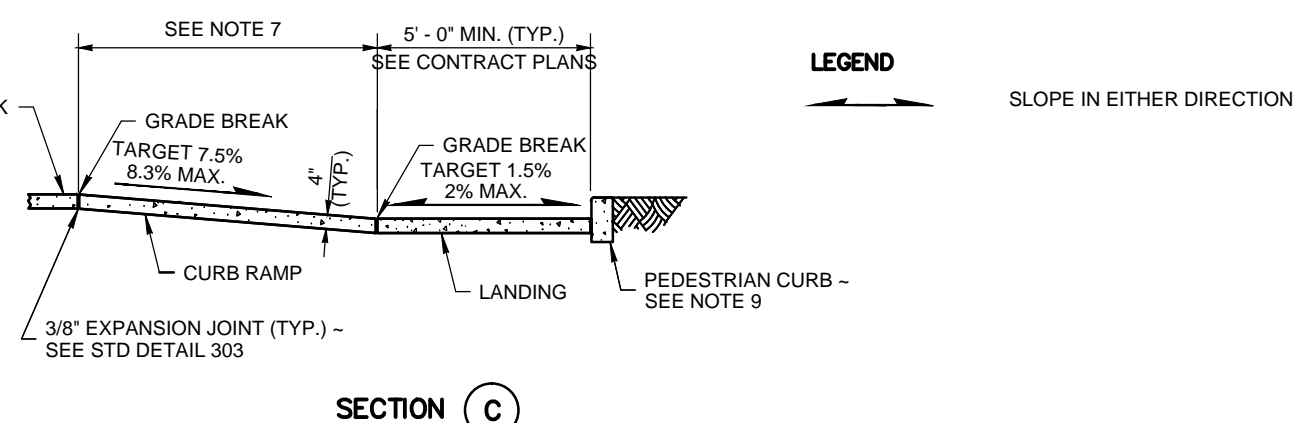
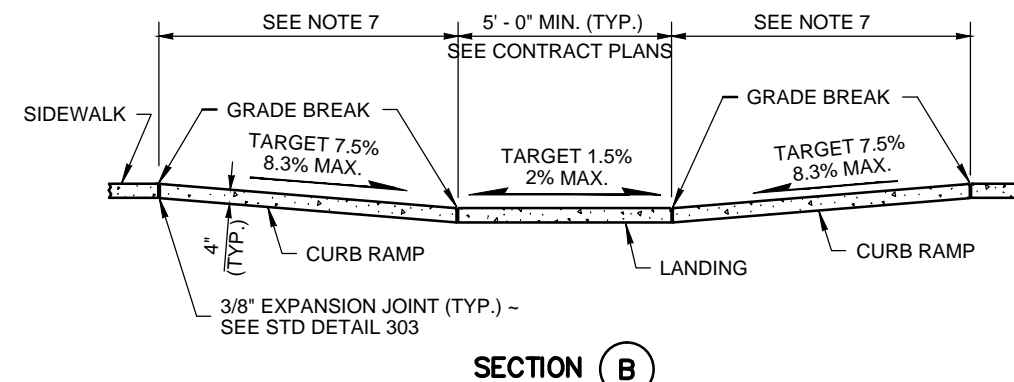
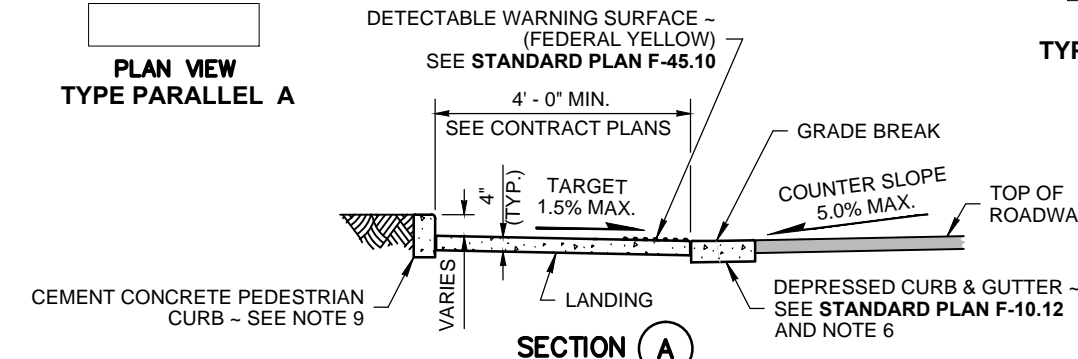
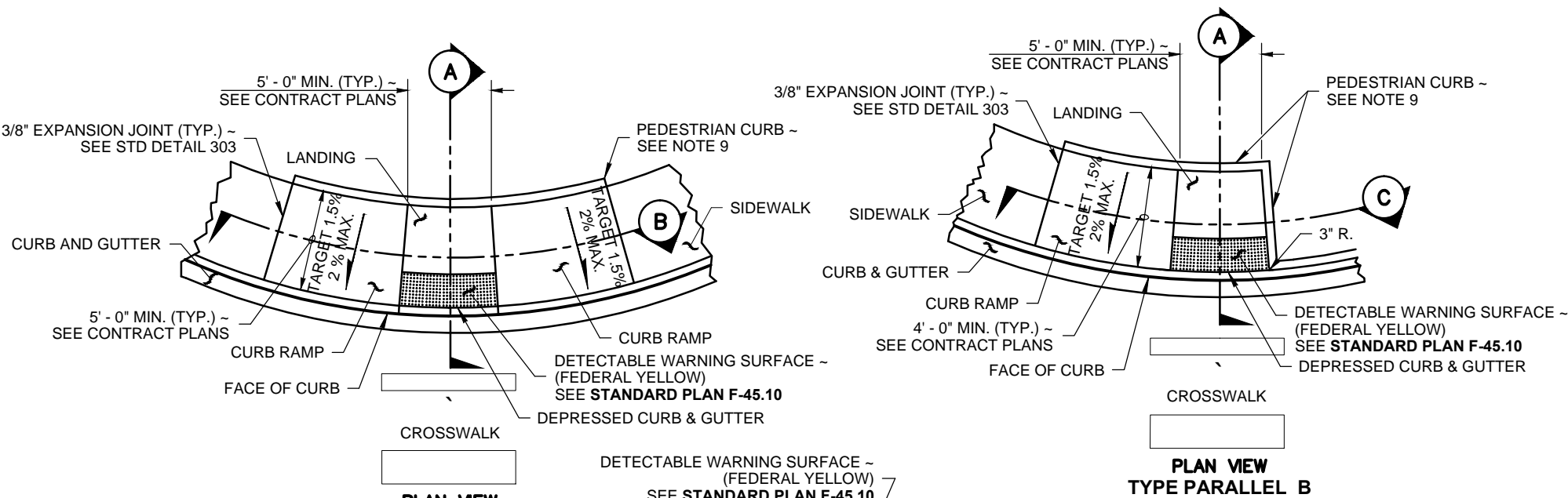
City of Redmond
WASHINGTON

STANDARD DETAILS

TYPE "2" DRIVEWAY

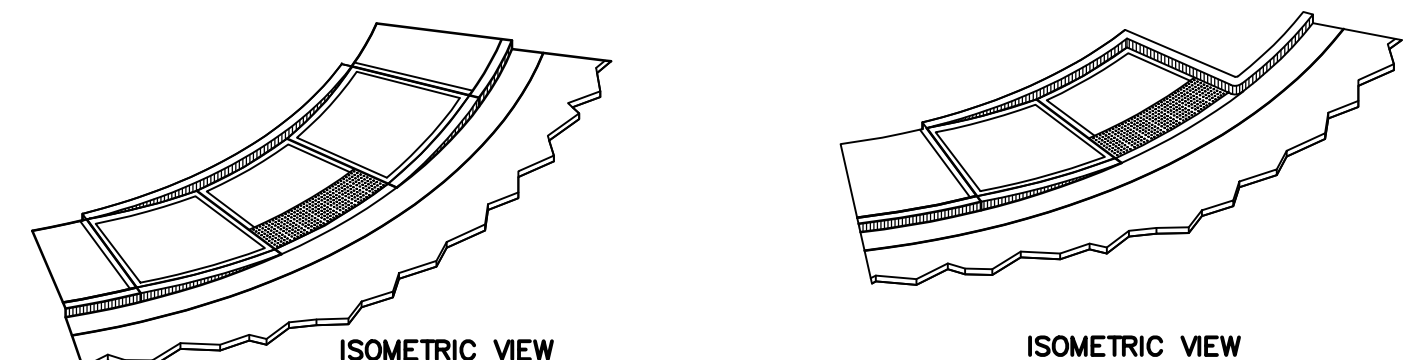
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DETAIL NUMBER: 306



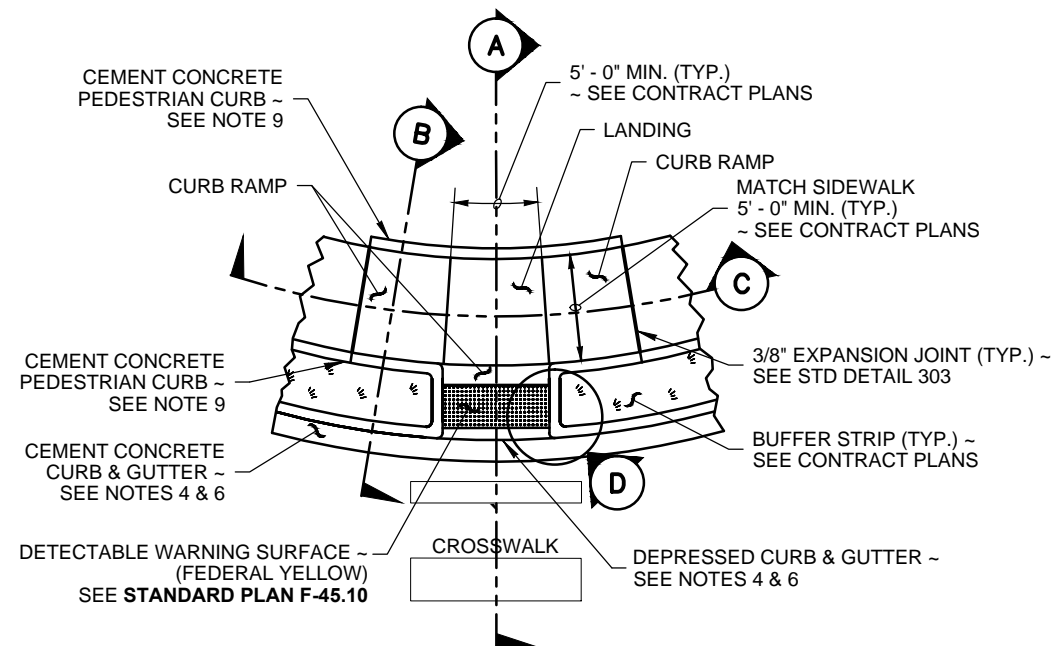
LEGEND

↔ SLOPE IN EITHER DIRECTION

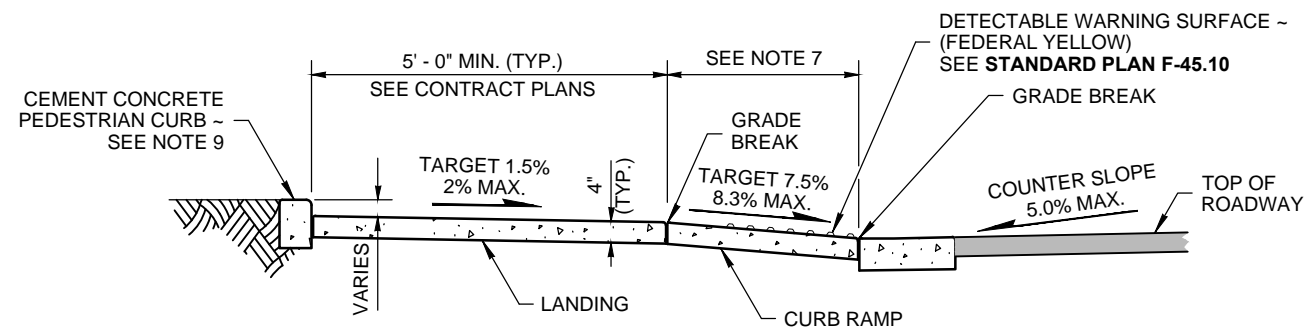


- NOTES**
1. Provide a separate Curb Ramp for each marked or unmarked crosswalk. Curb Ramp location shall be placed within the width of the associated crosswalk, or as shown in the Contract Plans.
 2. Where "GRADE BREAK" is called out, the entire length of the grade break between the two adjacent surface planes shall be flush.
 3. Do not place Gratings, Junction Boxes, Access Covers, or other appurtenances in front of the Curb Ramp or on any part of the Curb Ramp or Landing.
 4. See Contract Plans for the curb design specified. See STD Detail 304 and 304A for Curb, Curb and Gutter, Depressed Curb and Gutter, and Pedestrian Curb details.
 5. See **Standard Plan F-30.10** for Cement Concrete Sidewalk Details. See Contract Plans for width and placement of sidewalk.
 6. (Intentionally left BLANK)
 7. Ramp slope must be Target 7.5 percent (8.3 percent Max) or ramp length must exceed 15 feet.
 8. Curb Ramp, Landing, and Flares shall receive broom finish. See **Standard Specifications 8-14**.
 9. Pedestrian Curb may be omitted if the ground surface at the back of the Curb Ramp and/or Landing will be at the same elevation as the Curb Ramp or Landing and there will be no material to retain.

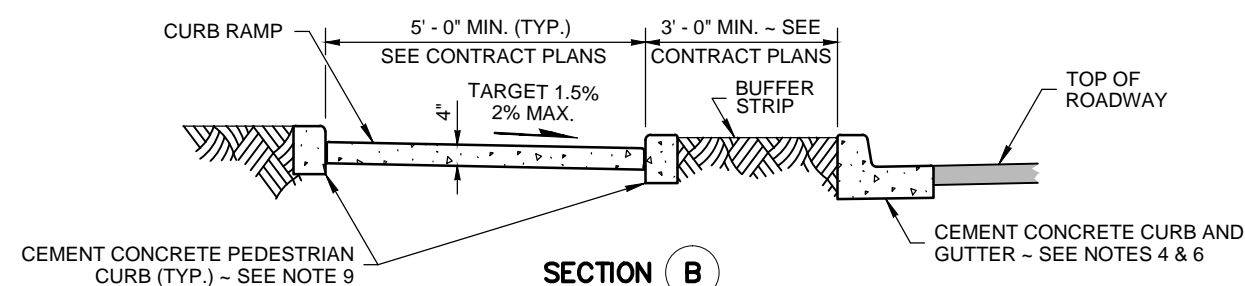
MODIFIED
STANDARD PLAN F-40.12-02
WSDOT 6/20/13
SHEET 1 OF 1 SHEET



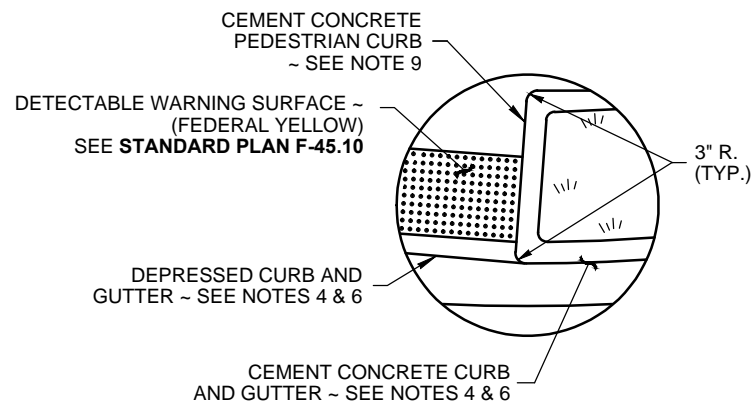
PLAN VIEW
TYPE COMBINATION
WITH BUFFER



SECTION A



SECTION B



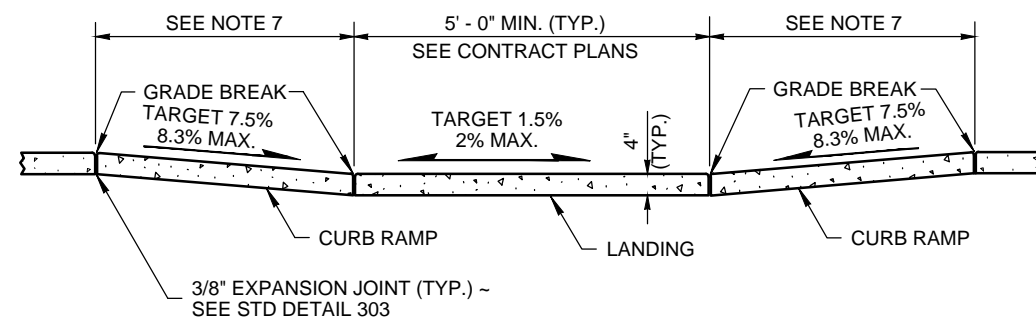
DETAIL D
CURB RADIUS DETAIL

NOTES

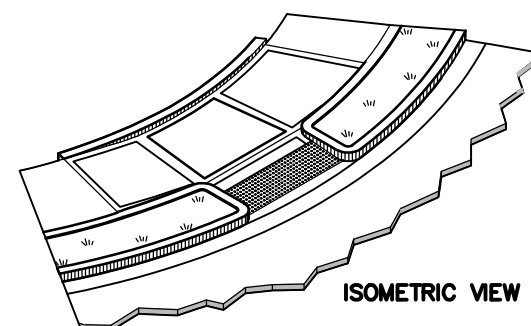
1. Provide a separate Curb Ramp for each marked or unmarked crosswalk. Curb Ramp location shall be placed within the width of the associated crosswalk or as shown in the Contract Plans.
2. Where "GRADE BREAK" is called out, the entire length of the grade break between the two adjacent surface planes shall be flush.
3. Do not place Gratings, Junction Boxes, Access Covers, or other appurtenances in front of the Curb Ramp or on any part of the Curb Ramp or Landing.
4. See Contract Plans for the curb design specified. See STD Detail 304 and 304A for Curb, Curb and Gutter, Depressed Curb, Gutter and Pedestrian Curb details.
5. See STD Detail 303 for Cement Concrete Sidewalk Details. See Contract Plans for width and placement of sidewalk.
6. (Intentionally left BLANK)
7. Ramp slope must be Target 7.5 percent (8.3 percent Max) or ramp length must exceed 15 feet.
8. Curb Ramp, Landing and Flares shall receive broom finish. See **Standard Specifications 8-14**.
9. Pedestrian Curb may be omitted if the ground surface at the back of the Curb Ramp and/or Landing will be at the same elevation as the Curb Ramp or Landing and there will not be material to retain.

LEGEND

— SLOPE IN EITHER DIRECTION



SECTION C



MODIFIED
STANDARD PLAN F-40.14-02
WSDOT 6/20/13
SHEET 1 OF 1 SHEET

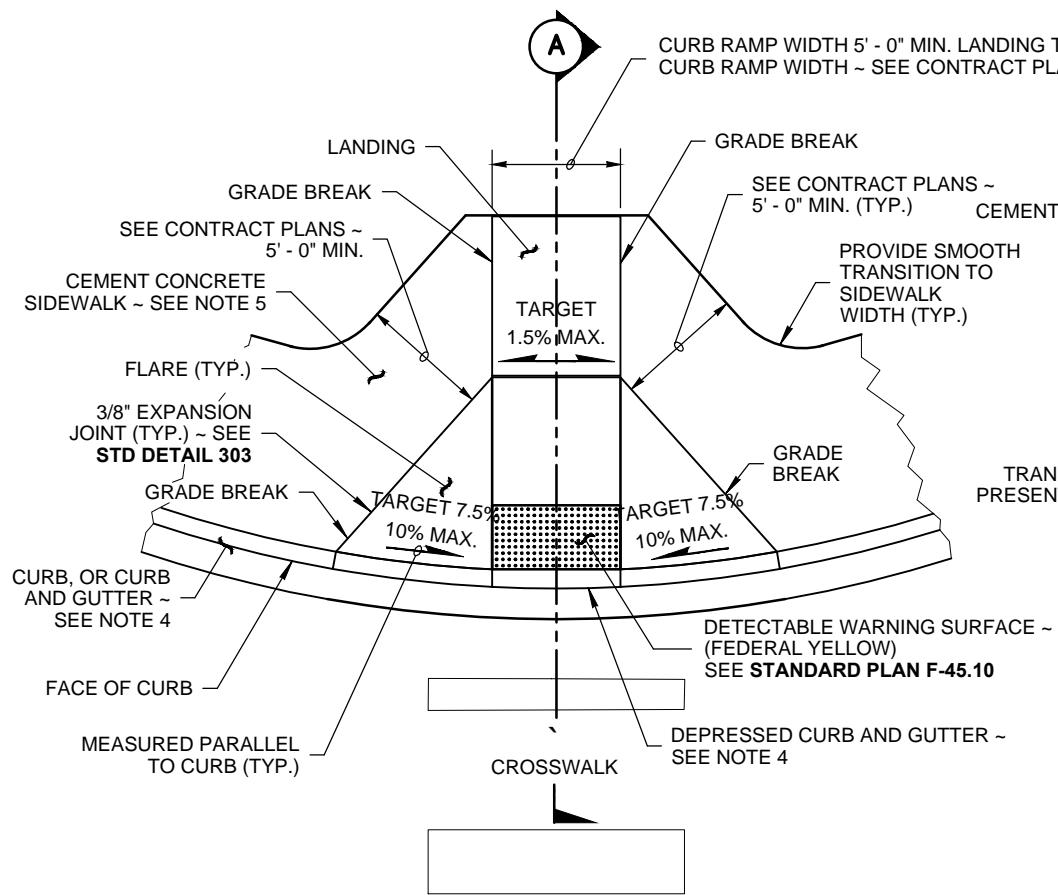
Rob Crittenden
APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER
REVISION DATE: JULY 01, 2015



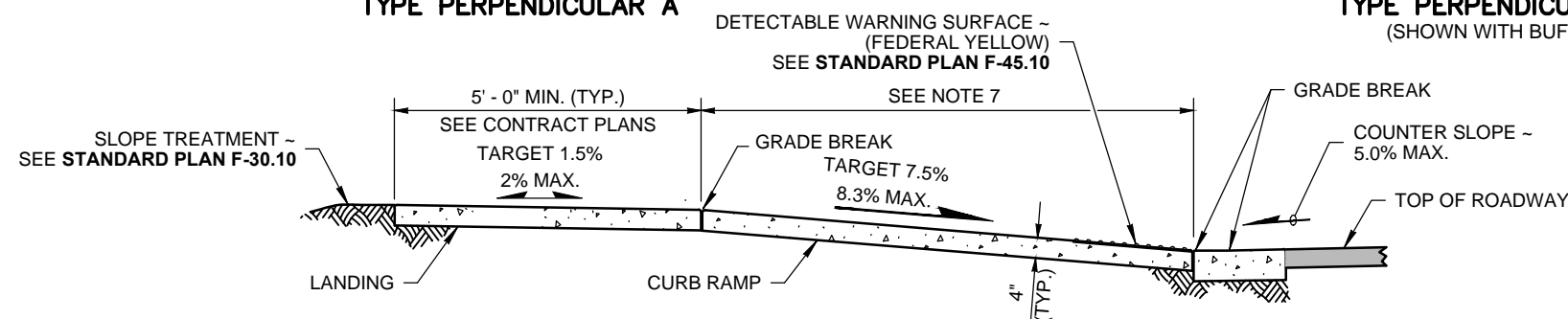
STANDARD DETAILS

COMBINATION
CURB RAMP

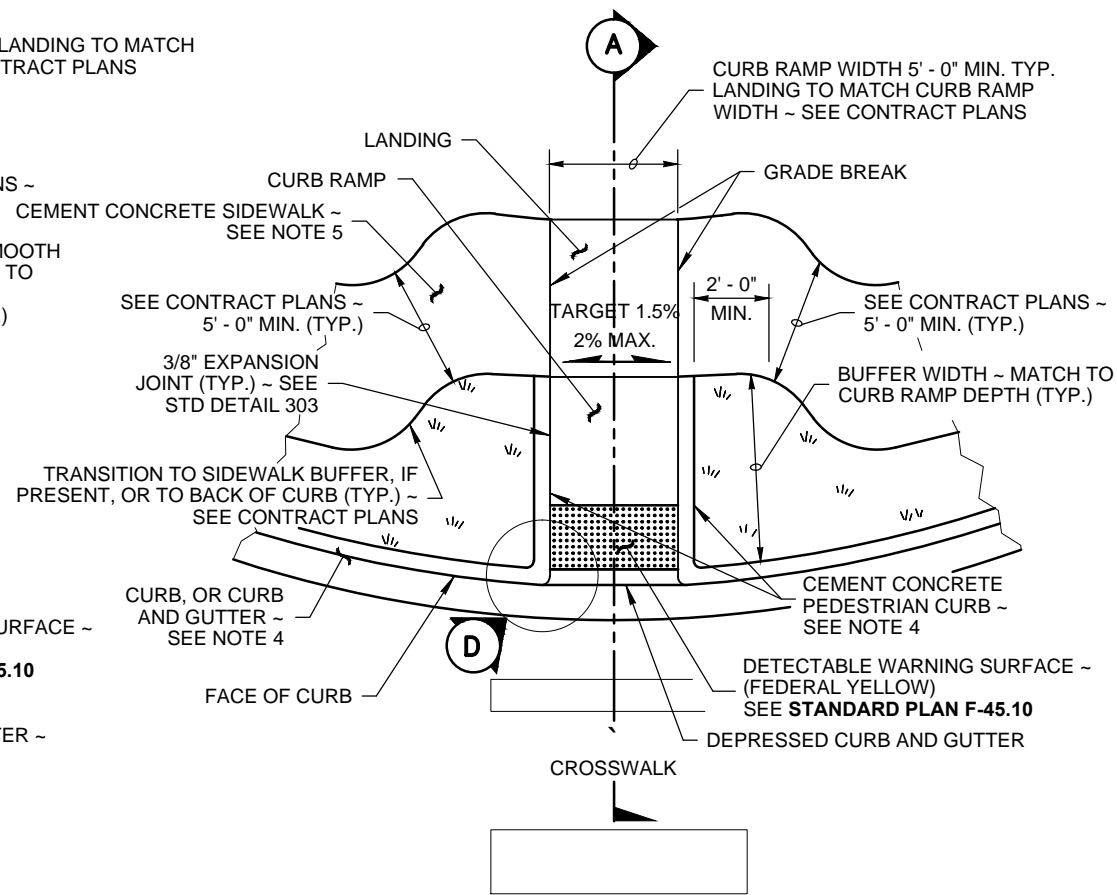
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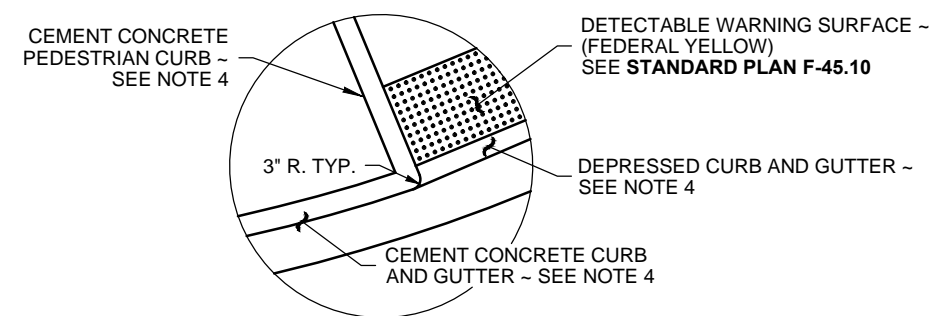
PLAN VIEW
TYPE PERPENDICULAR A



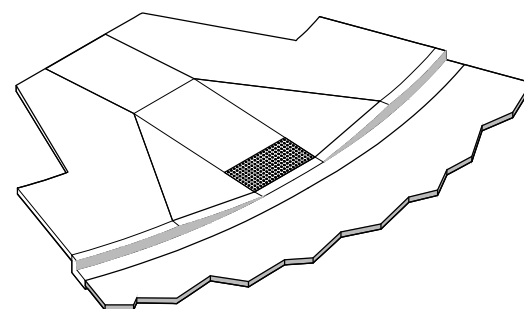
SECTION A



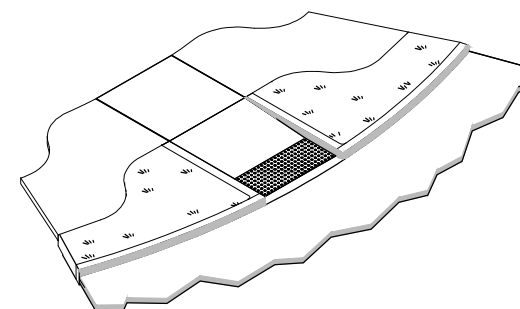
PLAN VIEW
TYPE PERPENDICULAR B
(SHOWN WITH BUFFER)



CURB RADIUS DETAIL D



ISOMETRIC VIEW



ISOMETRIC VIEW

NOTES

1. Provide a separate Curb Ramp for each marked or unmarked crosswalk. Curb Ramp location shall be placed within the width of the associated crosswalk or as shown in the Contract Plans.
2. Where "GRADE BREAK" is called out, the entire length of the grade break between the two adjacent surface planes shall be flush.
3. Do not place Gratings, Junction Boxes, Access Covers, or other appurtenances in front of the Curb Ramp or on any part of the Curb Ramp or Landing.
4. See the Contract Plans for the curb design specified. See STD Detail 304 and 304A for Curb, Curb and Gutter, Depressed Curb and Gutter, and Pedestrian Curb details.
5. See STD Detail 303 for Cement Concrete Sidewalk details. See Contract Plans for width and placement of sidewalk.
6. (Intentionally left BLANK)
7. Ramp slope must be Target 7.5 percent (8.3 percent Max) or ramp length must exceed 15 feet.
8. Curb Ramp, Landing, and Flares shall receive broom finish. See **Standard Specifications 8-14.**

LEGEND
SLOPE IN EITHER DIRECTION

MODIFIED
STANDARD PLAN F-40.15
WSDOT 6/20/13
SHEET 1 OF 1 SHEET

STANDARD DETAILS

PERPENDICULAR
CURB RAMP

FILE NAME: SD309.DWG DETAIL NUMBER: 309

Rob Crittenden

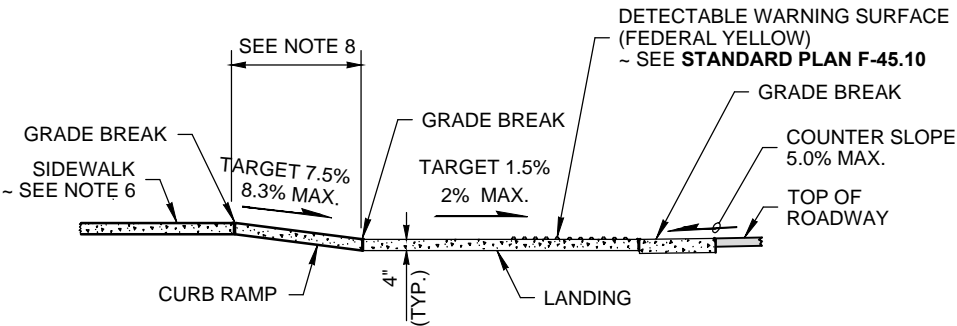
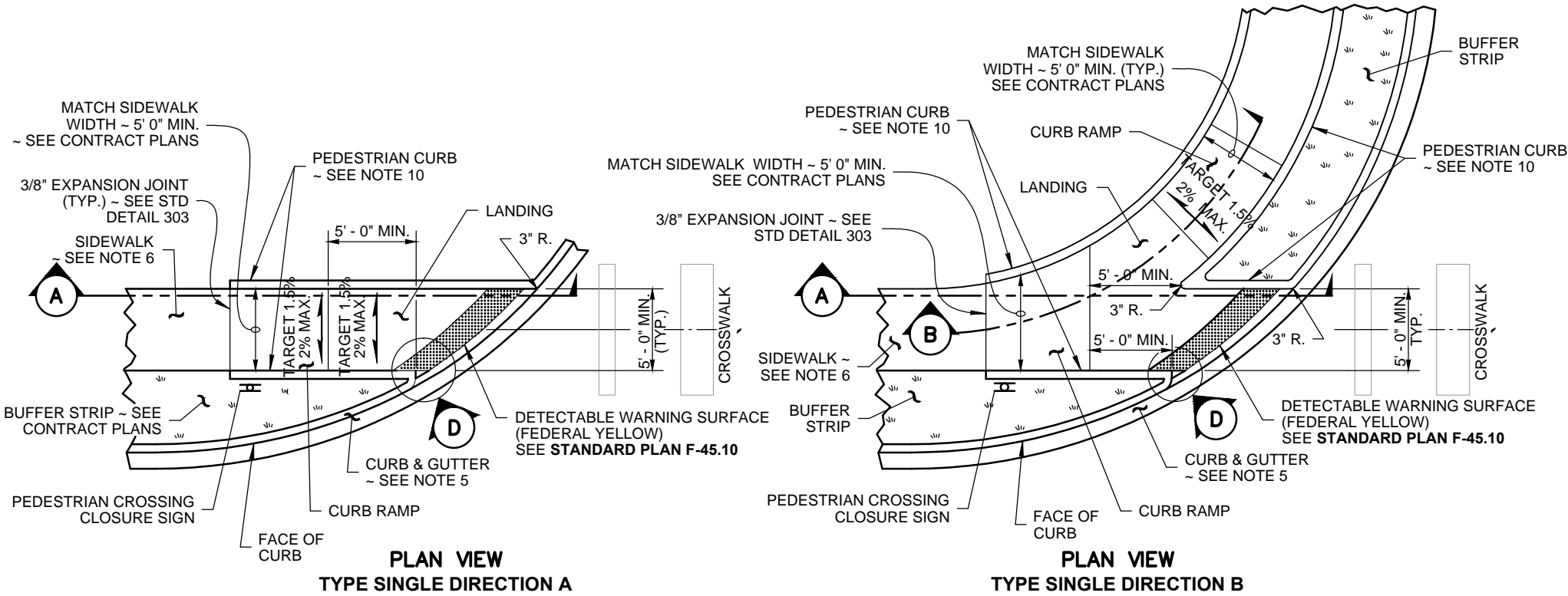
APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2018

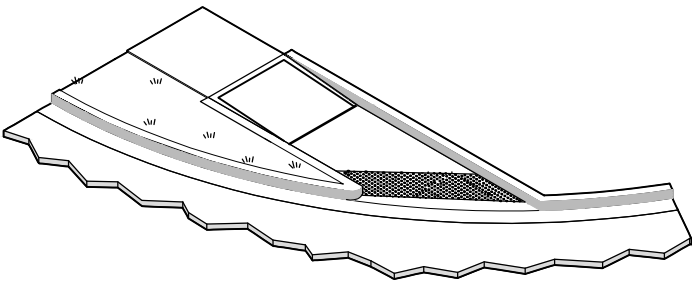


NOTES

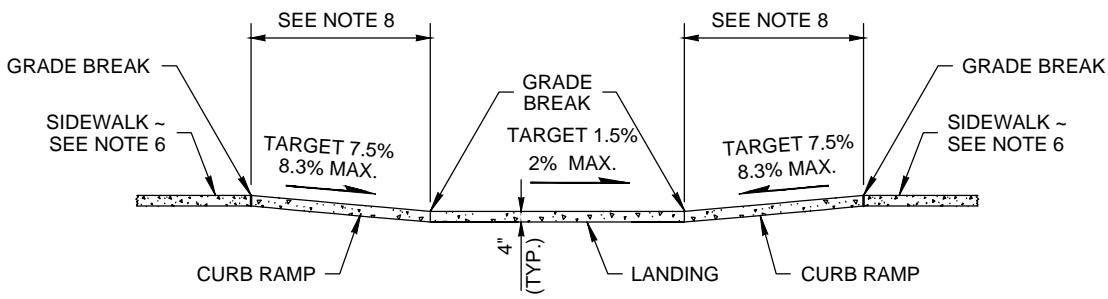
- 1. This plan is to be used where pedestrian crossing in one direction is not permitted.
- 2. Curb ramp location shall be placed within the width of the associated crosswalk, or as shown in the Contract Plans.
- 3. Where "GRADE BREAK" is called out, the entire length of the grade break between the two adjacent surface planes shall be flush.
- 4. Do not place gratings, junction boxes, access covers or other appurtenances in front of the curb ramp or on any part of the curb ramp or landing.
- 5. See the Contract Documents for the curb design specified. See STD Detail 304 and 304A for Curb, Curb and Gutter, Depressed Curb and Gutter and Pedestrian Curb details.
- 6. See STD Detail 303 for Cement Concrete Sidewalk Details. See contract plans for width and placement of sidewalk.
- 7. (Intentionally left BLANK)
- 8. Ramp slope must be Target 7.5 percent (8.3 percent Max)) or ramp length must exceed 15 feet.
- 9. Curb ramps and landings shall receive broom finish. See **Standard Specifications 8-14**.
- 10. Pedestrian Curb may be omitted if the ground surface at the back of the Curb Ramp and/or landing will be at the same elevation as the Curb Ramp or Landing and there will be no material to retain.



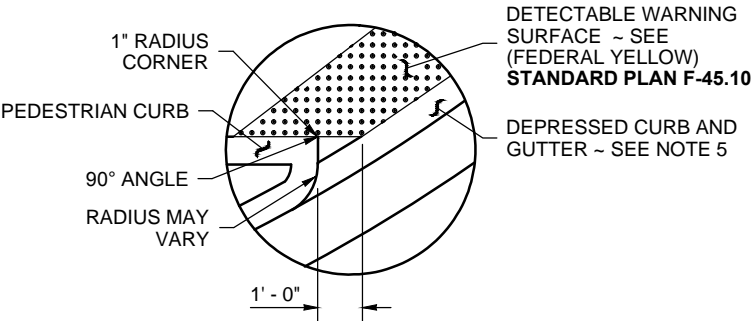
SECTION A



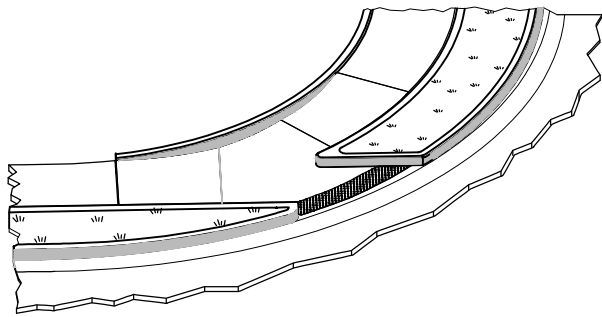
ISOMETRIC VIEW



SECTION B



DETAIL D



ISOMETRIC VIEW

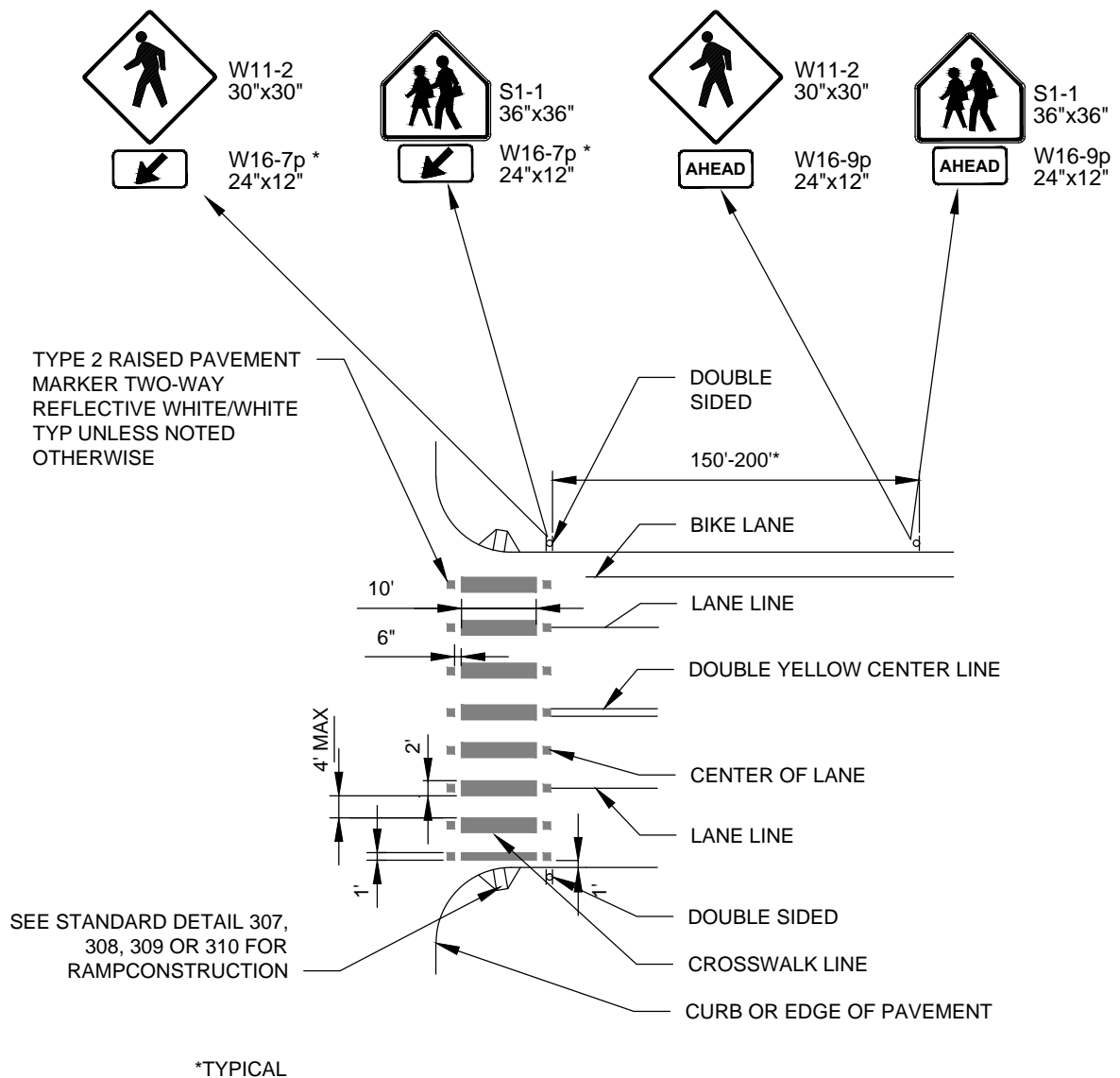
LEGEND
SLOPE IN EITHER DIRECTION

MODIFIED
STANDARD PLAN F-40.16-02
WSDOT 6/20/13
SHEET 1 OF 1 SHEET

Rob Crittenden
APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER
REVISION DATE: JULY 01, 2015



STANDARD DETAILS	
SINGLE DIRECTION CURB RAMP	
FILE NAME: SD310.DWG	DETAIL NUMBER: 310



UNPROTECTED CROSSWALK OR
UNSIGNALIZED SCHOOL CROSSWALK
 NTS

NOTES:

1. USE WHITE THERMOPLASTIC UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
2. SIGNS HAVE FLUORESCENT YELLOW GREEN BACKGROUND WITH BLACK LEGEND AND BORDER. SIGN MATERIAL: 3M DIAMOND GRADE DG3.
3. UNPROTECTED NON-SCHOOL CROSSWALK: USE W11-2 WITH W16-7p AND W11-2 WITH W16-9P.
4. UNPROTECTED SCHOOL CROSSWALK USE S1-1 WITH W16-7p AND S1-1 WITH W16-9P.
5. PROTECTED CROSSWALK OMIT S1-1 WITH W16-7P AND S1-1 WITH W16-9P.

Rob Crittenden

APPROVED BY: ROB CRITTENDEN
 TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015

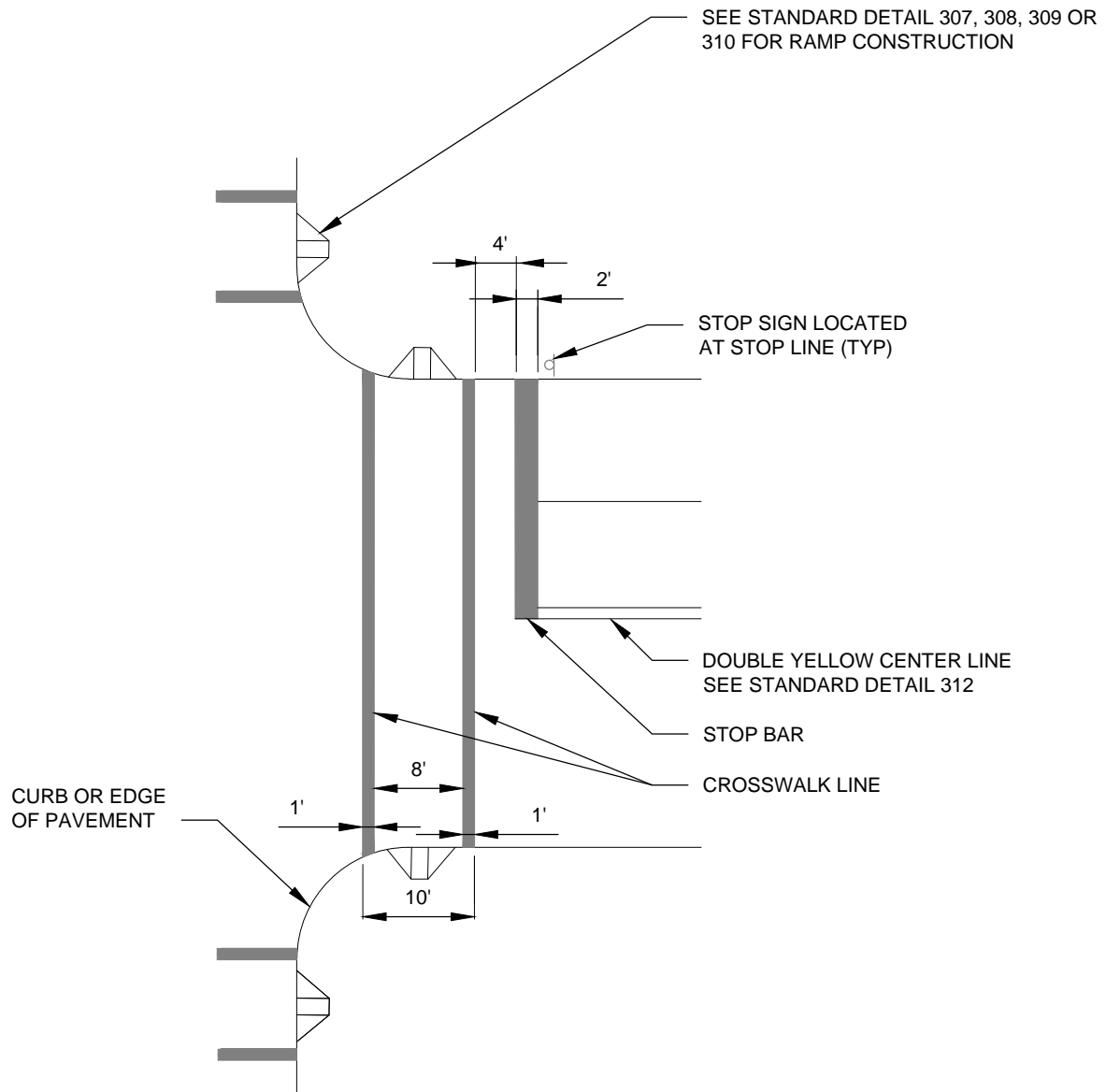


STANDARD DETAILS

CROSSWALK MARKINGS & SIGNS

FILE NAME: SD311.DWG

DETAIL NUMBER: 311



PLAN VIEW

NTS

NOTES:

1. USE WHITE THERMOPLASTIC UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
2. INSTALLATION OF OFFSET STOP BAR SHALL BE ENGINEER-APPROVED

Rob Crittenden

APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2018

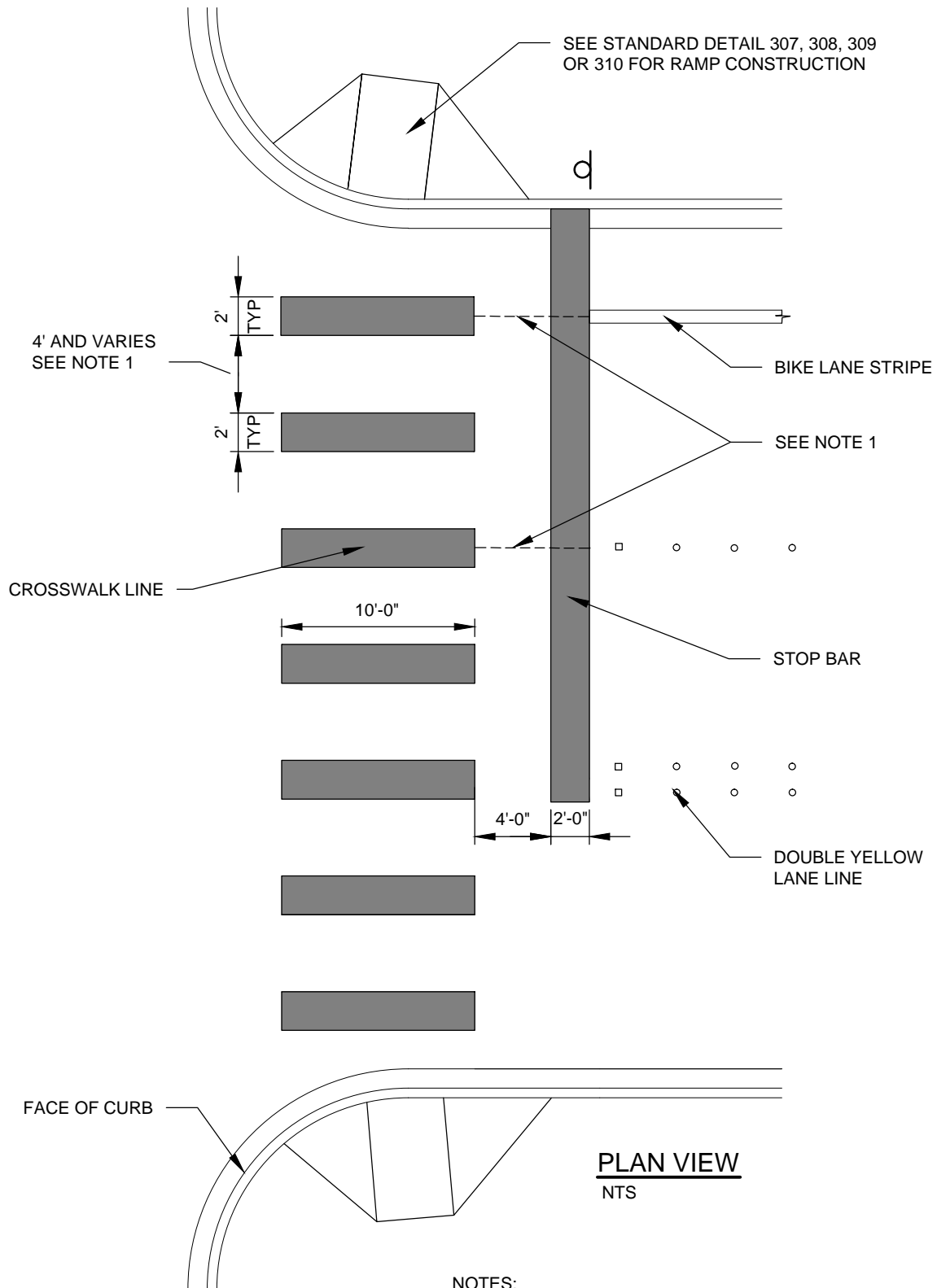


City of Redmond
WASHINGTON

STANDARD DETAILS

SIGNALIZED CROSSWALK
& STOP BAR

FILE NAME: SD311A.DWG DETAIL NUMBER: 311A



NOTES:

1. CENTER CROSSWALK BARS ON LANE LINES AND AT CENTER OF LANES AS SHOWN IN ORDER TO AVOID TIRE WEAR.
2. USE WHITE THERMOPLASTIC UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
3. INSTALLATION OF OFFSET STOP BAR SHALL BE ENGINEER-APPROVED.

Rob Crittenden

APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2018



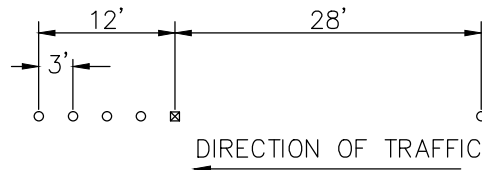
STANDARD DETAILS

URBAN CENTER CROSSWALK &
STOP BAR

FILE NAME: SD311B

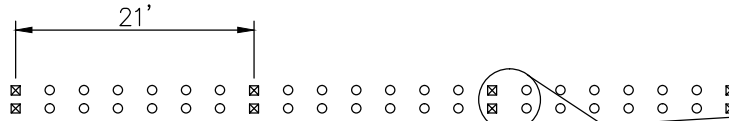
DETAIL NUMBER: 311B

TYPE A
LANE LINE

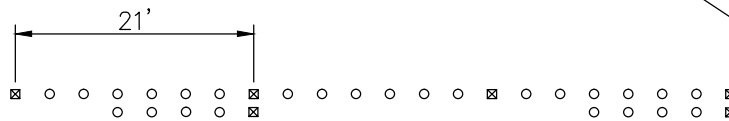


- WSDOT TYPE 1
(4" YELLOW OR WHITE)
- ⊠ WSDOT TYPE 2
(4" YY OR 2W
REFLECTORIZED)

TYPE B
DOUBLE YELLOW
CENTER LINE



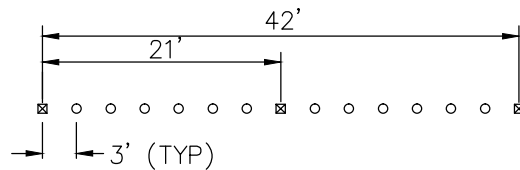
TYPE C
TWO-WAY LEFT TURN/
NO-PASS LINE



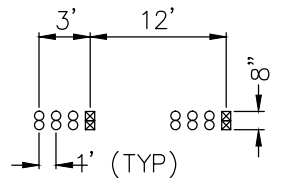
TYPE D
WIDE LINE



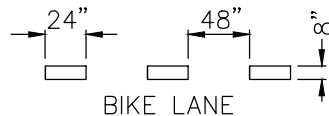
TYPE E
NO-PASS LINE



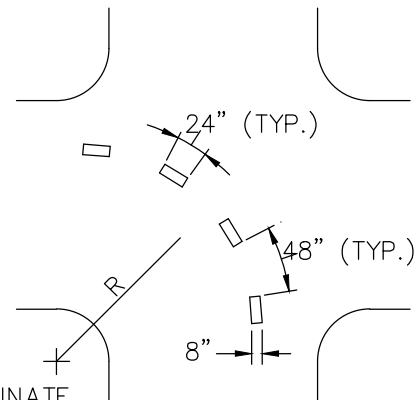
TYPE F
DROP LANE LINE



TYPE G
DOTTED WIDE LINE



COORDINATE
OR STA+OFF



DOTTED EXTENSION
LINE
THERMOPLASTIC

NOTES:

1. FOR RAISED PAVEMENT MARKERS USE HOT MELT POLYMER BASED BITUMINOUS ADHESIVE.
2. BIKE LANE STRIPES ARE 8" PAINT, OR AS DIRECTED BY THE ENGINEER.
3. EDGE LANE STRIPES ARE 4" PAINT, OR AS DIRECTED BY THE ENGINEER.
4. FOR TEMPORARY PAINTED CHANNELIZATION, RETRO-REFLECTIVE GLASS BEADS SHALL BE APPLIED TO THE PAINT DURING APPLICATION. TYPE II RAISED PAVEMENT MARKERS SHALL BE INSTALLED TO MATCH COLOR AND LINE TYPE AT 40' INTERVALS.

Rob Crittenden

APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2017



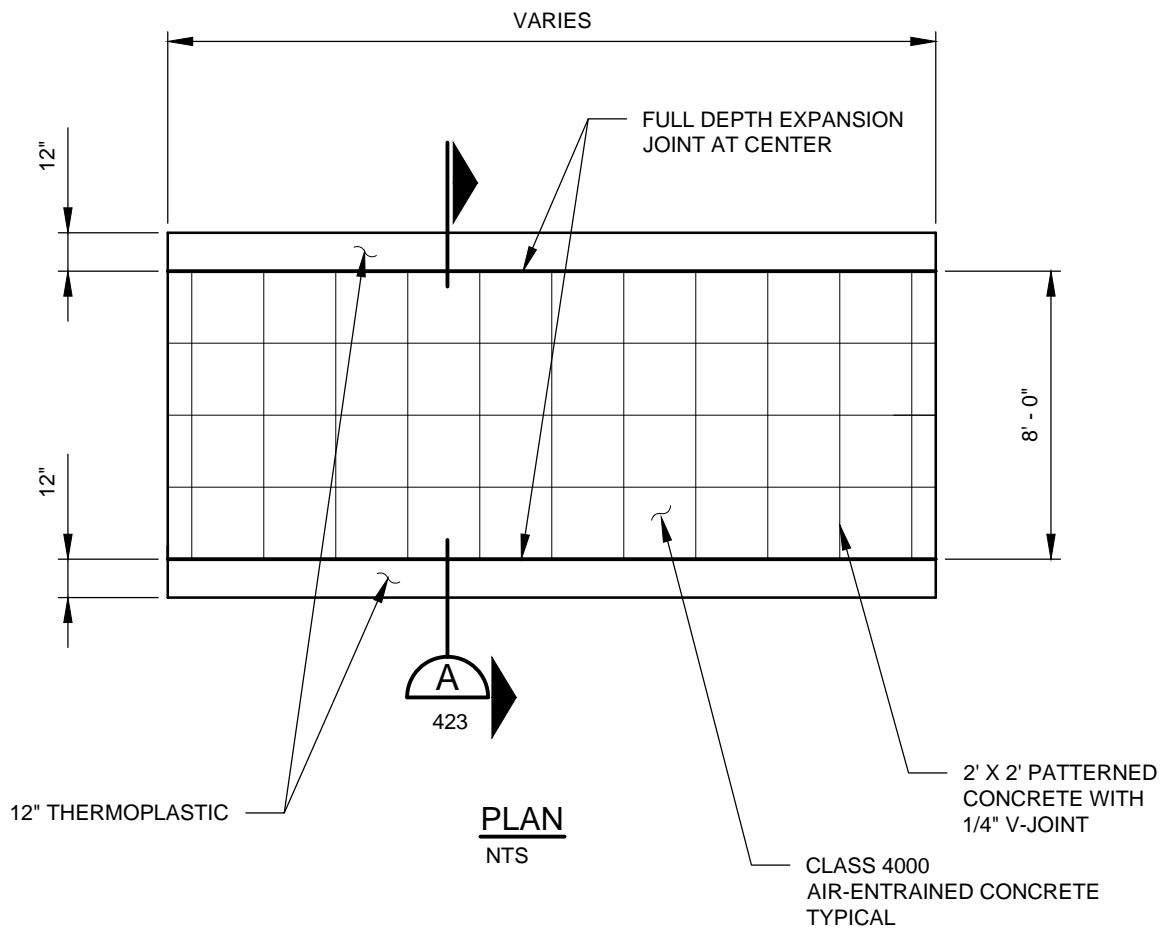
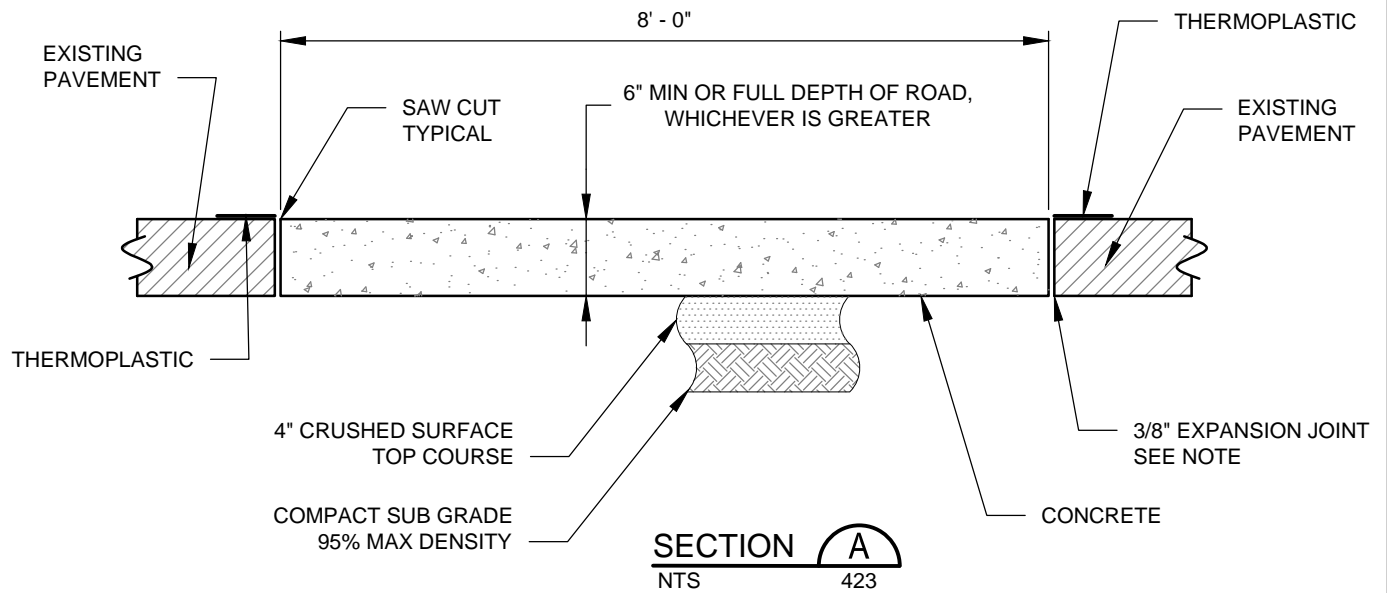
City of Redmond
WASHINGTON

STANDARD DETAILS

CHANNELIZATION
LANE LINE TYPES

FILE NAME: SD312.DWG

DETAIL NUMBER: 312



NOTE:

EXPANSION JOINT FULL DEPTH WHEN WALK IS ADJACENT TO AN EXISTING CONCRETE SHEET.

Donald Cairns

APPROVED BY: DONALD CAIRNS
TRANSPORTATION PLANNING AND ENGINEERING MANAGER

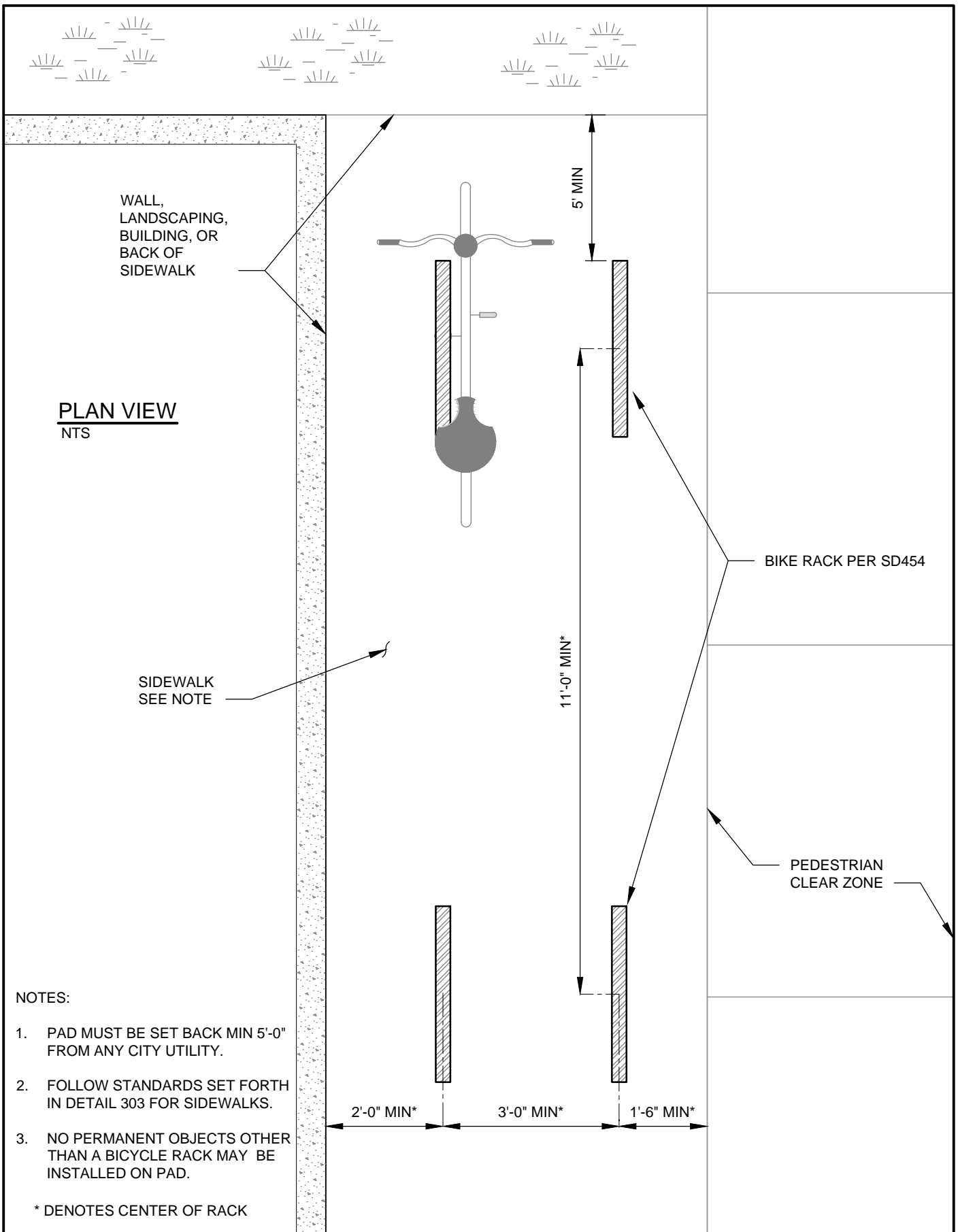
REVISION DATE: MARCH 01, 2018



STANDARD DETAILS

PATTERNED CONCRETE CROSSWALK

FILE NAME: SD313.DWG DETAIL NUMBER: 313



PLAN VIEW
NTS

IF THIS AREA IS LESS THAN
2'-0" WIDE THEN AREA MUST
BE CONCRETE.

BACK OF CURB

STREET

NOTES:

1. PAD MUST BE SET BACK MIN 5'-0" FROM ANY CITY UTILITY ACCESS.
2. FOLLOW STANDARDS SET FORTH IN DETAIL 303 FOR SIDEWALKS.
3. NO PERMANENT OBJECTS OTHER THAN A BICYCLE RACK MAY BE INSTALLED ON PAD.

* DENOTES CENTER OF RACK

PEDESTRIAN
CLEAR ZONE

2'-0" MIN SET BACK

4'-0" MIN*

3' MIN

4'-0" MIN*

1'-6" MIN SET BACK

8'-0" MIN X 4'-0" MIN
CONCRETE PAD.
SEE NOTES 1, 2 AND 3.

PLANTER STRIP/
FURNITURE ZONE

STANDARD DETAILS

ON STREET BICYCLE PARKING

REVISION DATE: MARCH 01, 2016

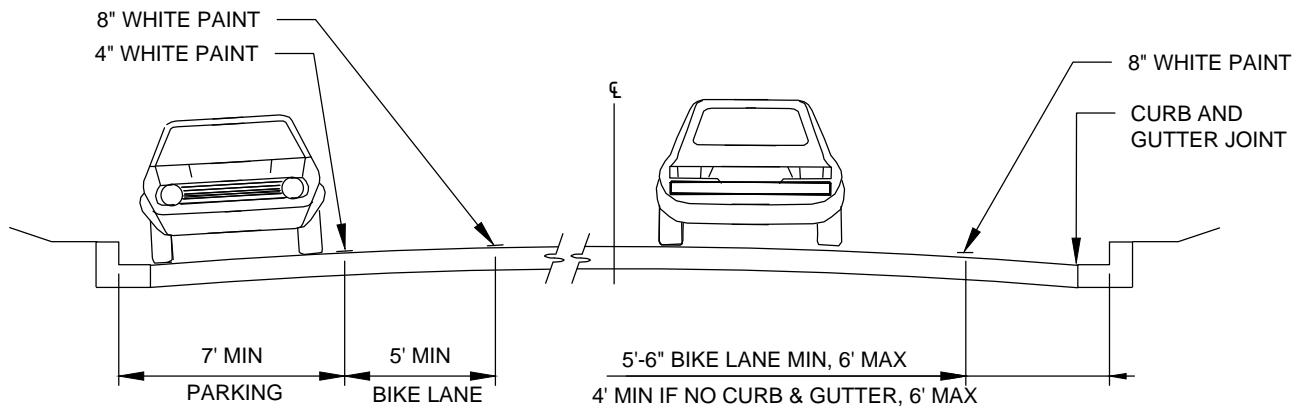


FILE NAME: SD318.DWG

DETAIL NUMBER: 318

APPROVED BY: DONALD CAIRNS
TRANSPORTATION PLANNING AND ENGINEERING MANAGER

Donald Cairns

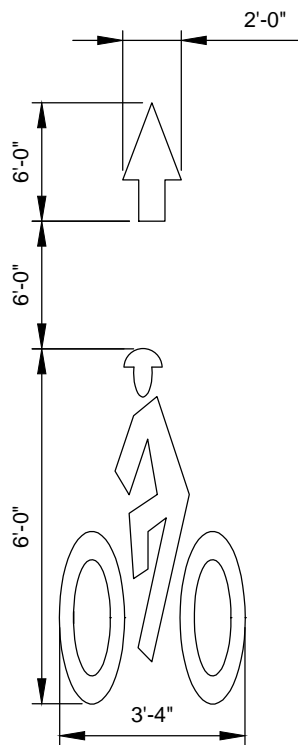


BICYCLE LANE WITH PARKING

NTS

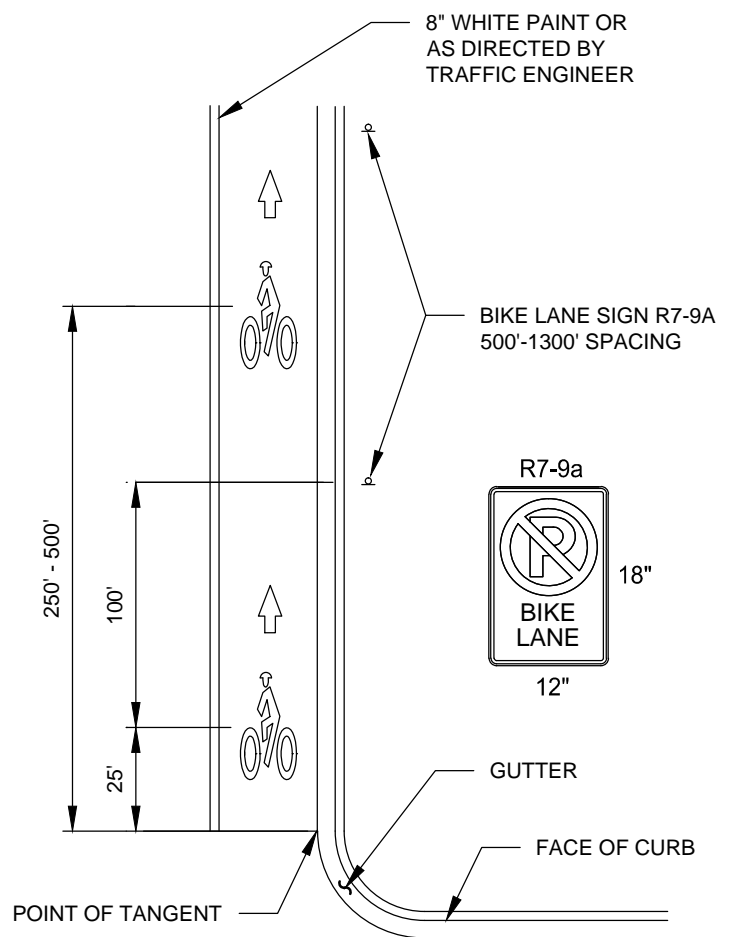
BICYCLE LANE WITHOUT PARKING

NTS



BICYCLE LANE SYMBOL

NTS



TYPICAL BICYCLE LANE

NTS

NOTES:

1. BIKE LANE SYMBOL AND ARROW MATERIAL SHALL BE 90 MILL, PERFORMED, SKID RESISTANT THERMOPLASTIC.
2. SIGN MATERIAL SHALL BE 3M DG3.
3. BICYCLE SYMBOL FACES CENTERLINE.

Rob Crittenden

APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



City of Redmond
WASHINGTON

STANDARD DETAILS

BICYCLE LANES

FILE NAME: SD319.DWG

DETAIL NUMBER: **319**



1. SIGN R3-17bP IS NOT TYPICALLY NEEDED. IF THIS SIGN IS USED, PAVEMENT MARKING SHALL BE INSTALLED ADJACENT TO THE SIGN.
2. FOR BIKE PAVEMENT MARKING DETAIL, SEE STANDARD DETAIL 319.

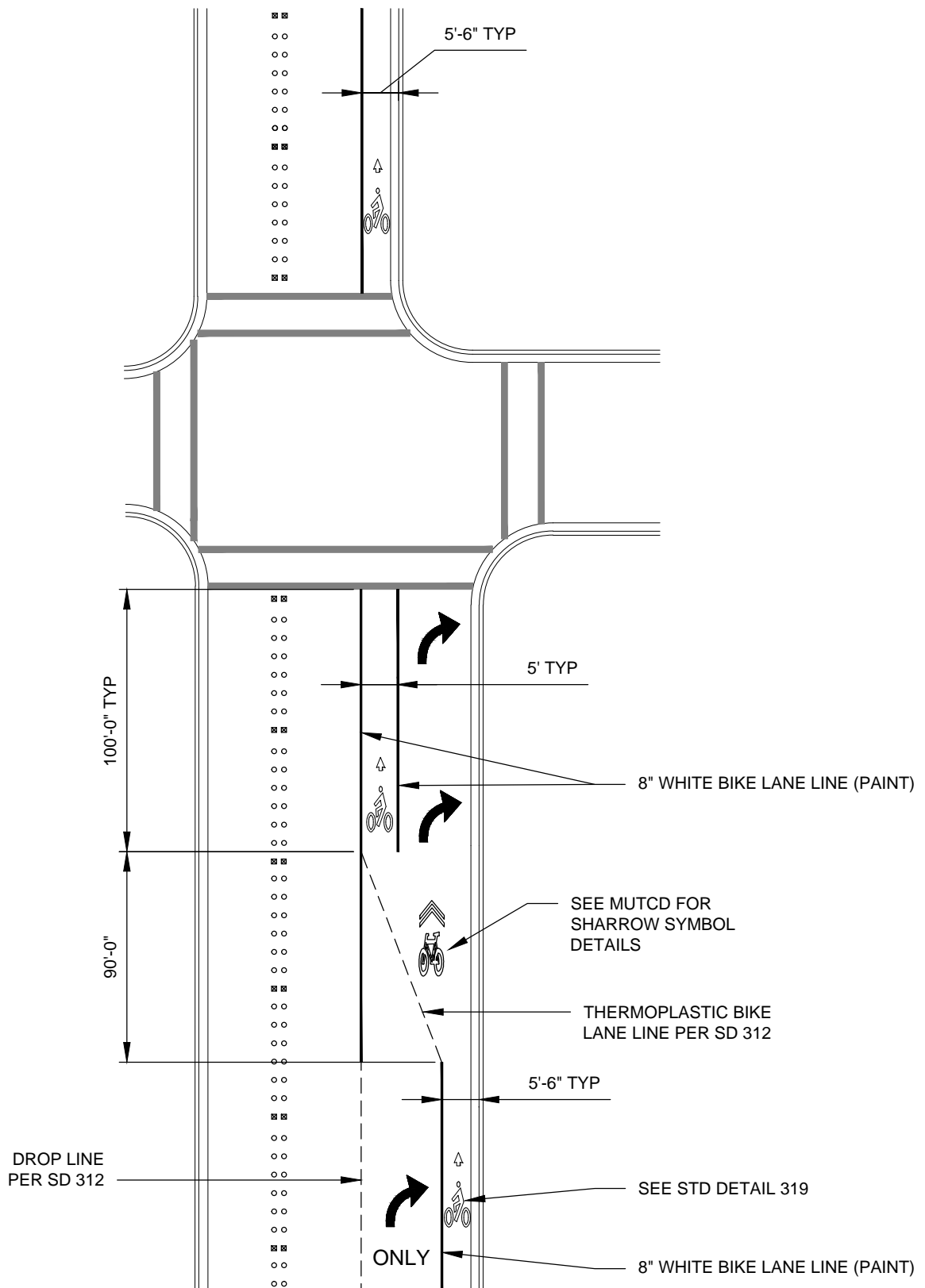
REVISION DATE: MARCH 01, 2016



STANDARD DETAILS

BICYCLE LANE CHANNELIZATION

FILE NAME: SD319A.DWG	DETAIL NUMBER: 319A
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Rob Crittenden

APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

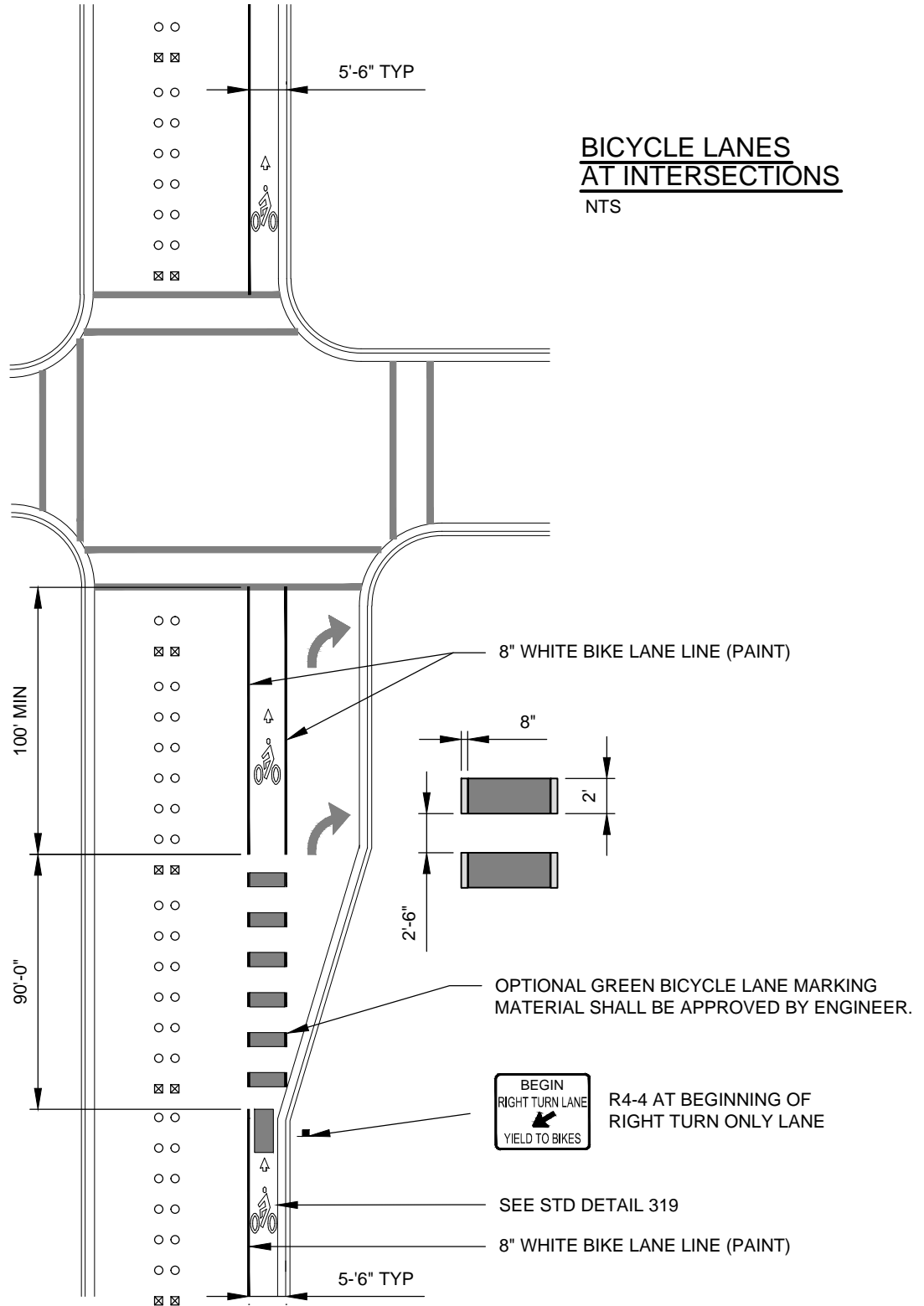
REVISION DATE: MARCH 01, 2016



STANDARD DETAILS

BIKE LANE TREATMENT AT DROP LANE RIGHT TURN

FILE NAME: SD319B.DWG | DETAIL NUMBER: 319B



Rob Crittenden

APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2016

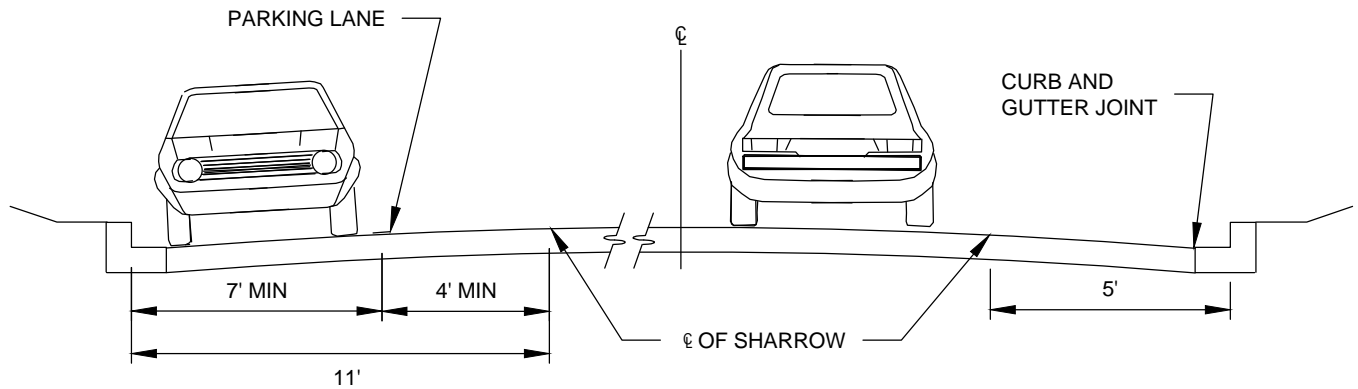


City of Redmond
WASHINGTON

STANDARD DETAILS

BICYCLE LANES
AT INTERSECTIONS

FILE NAME: SD319C.DWG DETAIL NUMBER: 319C

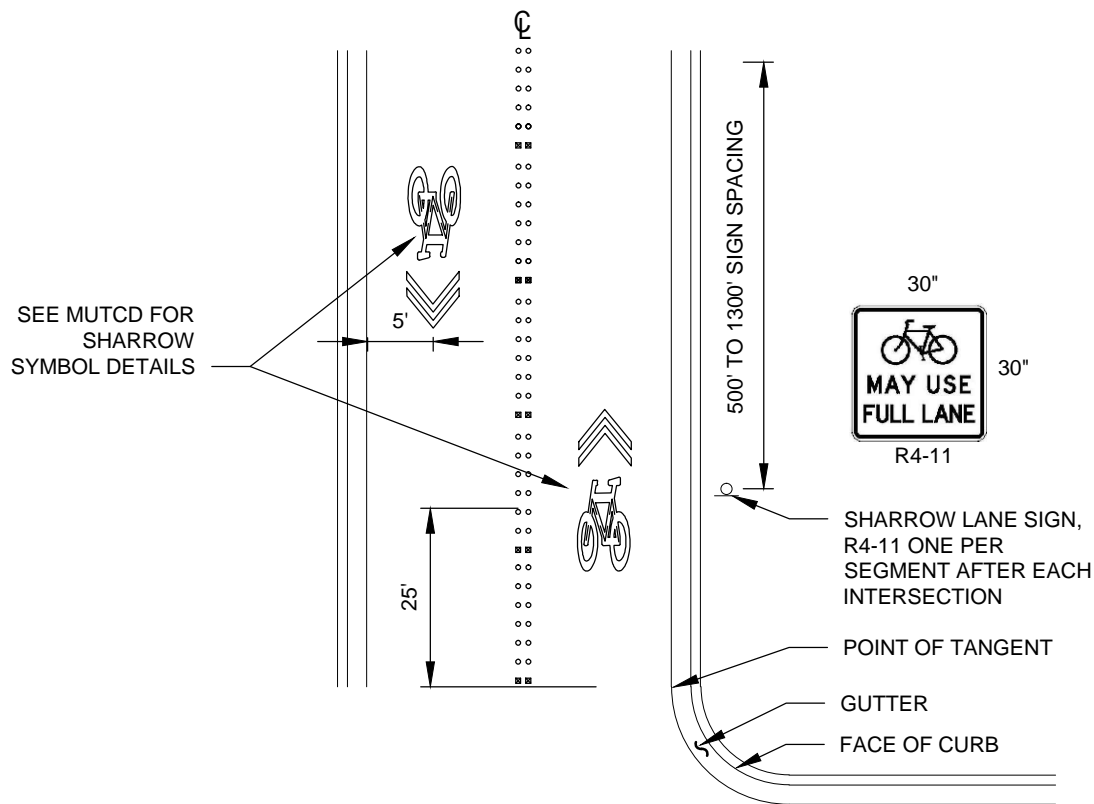


SHARROW LANE WITH PARKING

NTS

SHARROW LANE WITHOUT PARKING

NTS



TYPICAL SHARROW LAYOUT

NTS

NOTES:

1. ADJUSTMENT TO DIMENSIONS SHALL BE APPROVED BY THE ENGINEER.
2. MATERIAL SHALL BE PREFORMED SKID RESISTANT THERMOPLASTIC 90 MIL.
3. FOR LANE WIDTHS 14 FEET OR GREATER, THE SHARE LANE MARKINGS SHALL BE PLACED 2 FEET FROM THE FACE OF CURB.

Rob Crittenden

APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2016

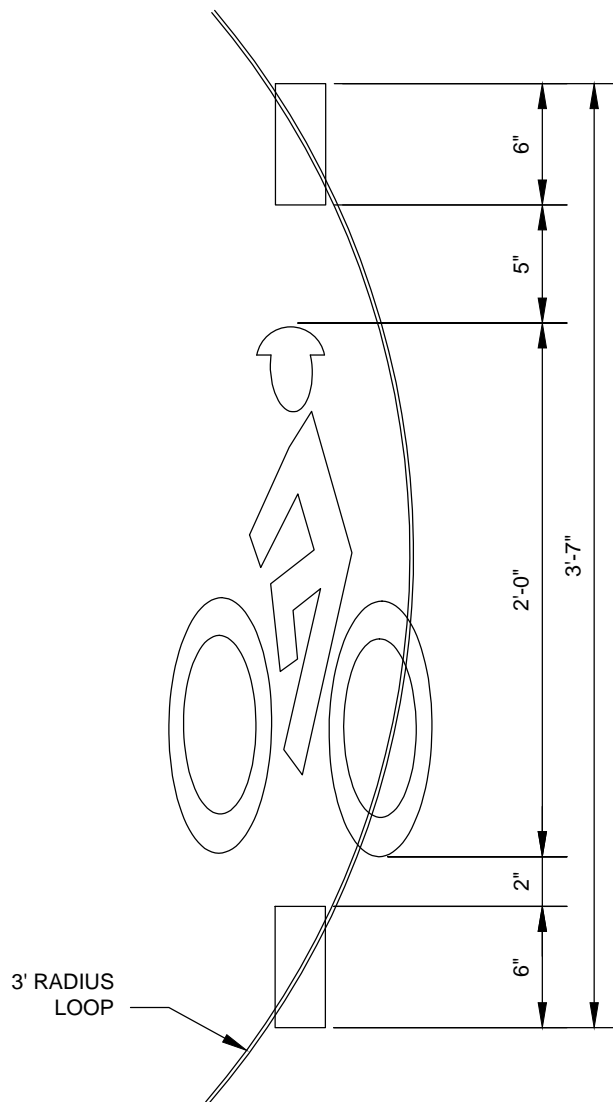
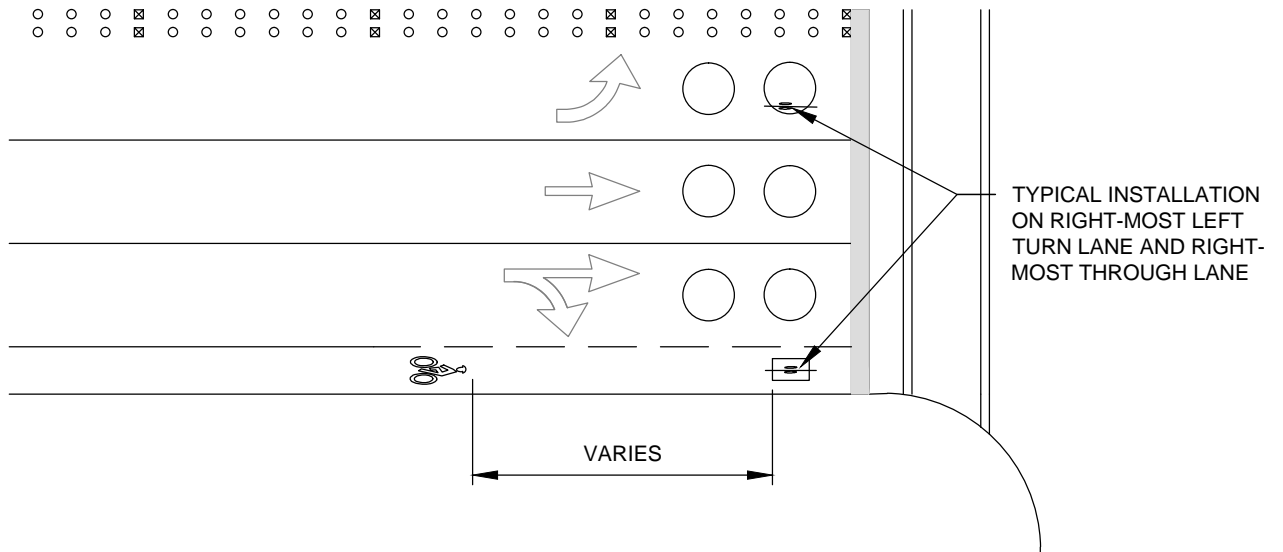


STANDARD DETAILS

SHARROW LANES

FILE NAME: SD320.DWG

DETAIL NUMBER: 320



NOTE:

MATERIAL SHALL BE PREFORMED, SKID RESISTANT, 90 MIL THERMOPLASTIC.

Rob Crittenden

APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

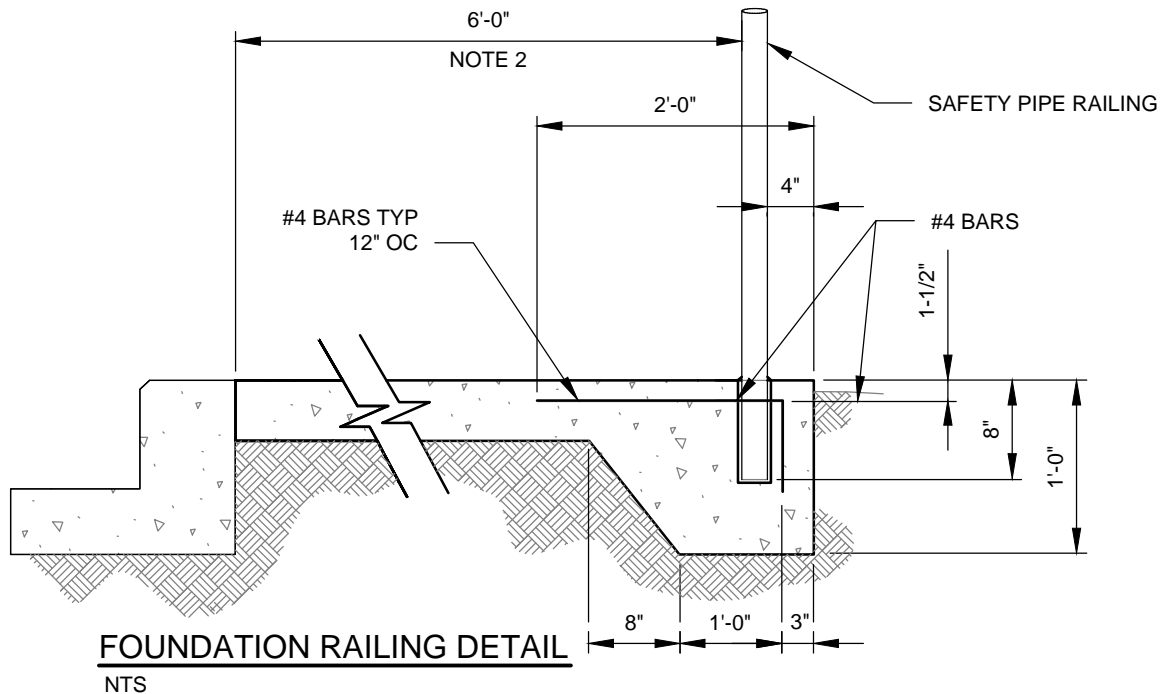
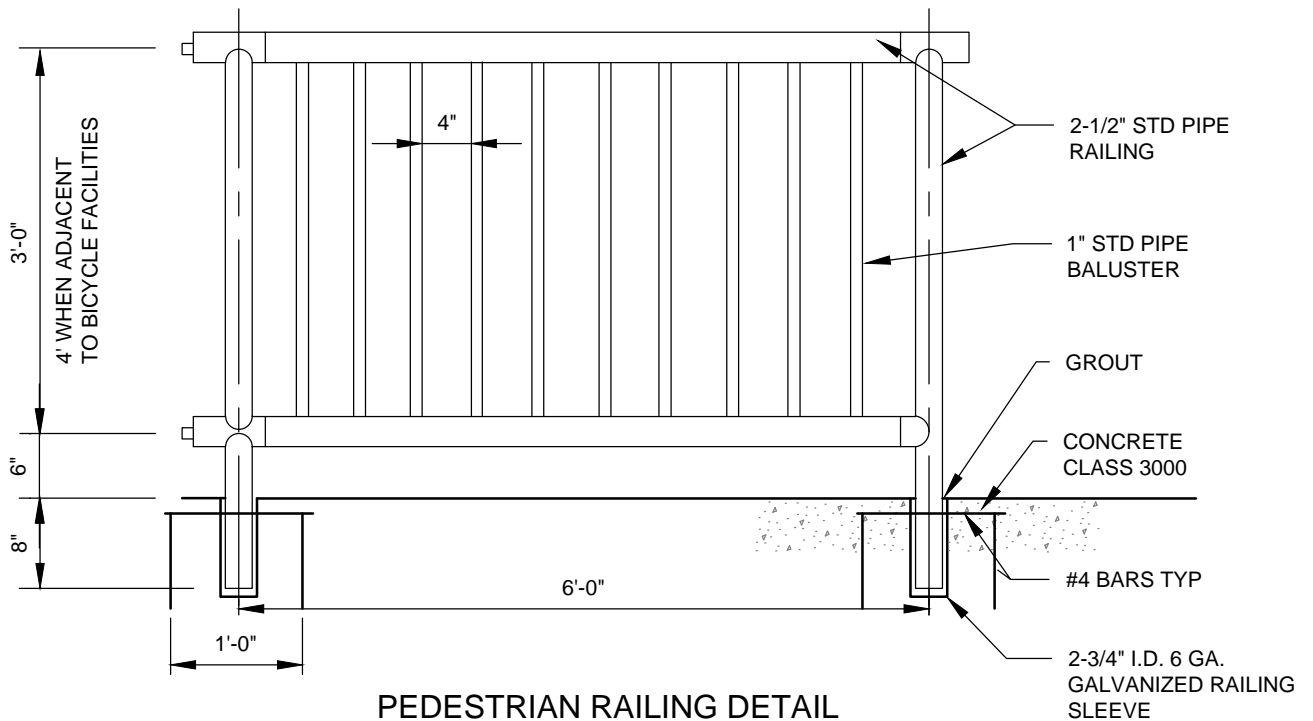
REVISION DATE: JULY 01, 2015



STANDARD DETAILS

**BICYCLE DETECTOR
PAVEMENT MARKING**

FILE NAME: SD320A.DWG DETAIL NUMBER: 320A



NOTES:

1. PIPES SHALL BE ALUMINUM AND MEET ASTM B241/B241M-02 OR B429-02 ALLOY 6063-T6 SCHEDULE 40 (STD PIPE), OR AS APPROVED BY CITY ENGINEER.
2. WIDTH PER RZC APPENDIX 2 IN TABLE 1 AND 2.

Lisa W. Rigg
APPROVED BY: LISA W. RIGG
DEVELOPMENT ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2018

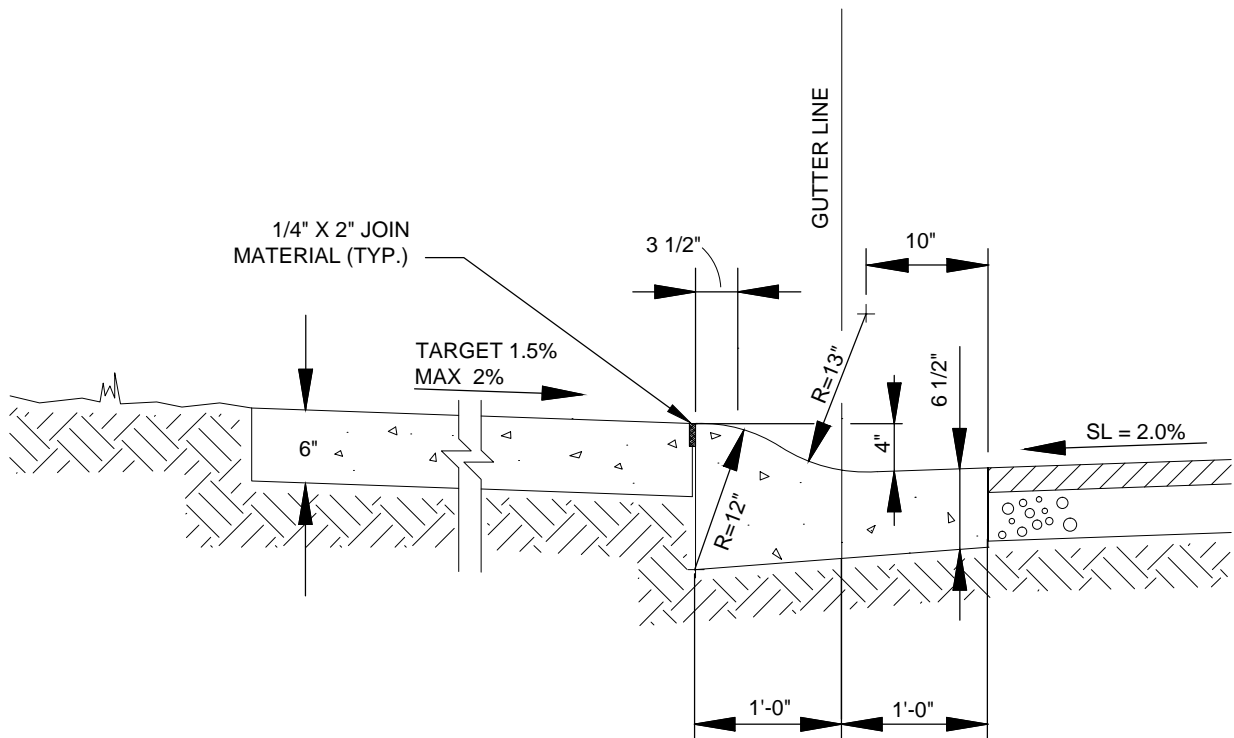


STANDARD DETAILS

TYPICAL PEDESTRIAN RAILING

FILE NAME: SD321.DWG

DETAIL NUMBER: 321



SECTION NTS

NOTES:

1. CONCRETE SHALL BE AIR ENTRAINED CLASS 3000.
2. ROLLED CURB AND GUTTER CAN BE USED ON CITY MAINTAINED STREETS ONLY FOR REPAIRS TO EXISTING ROLLED CURB AND GUTTER.

Rob Crittenden

APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2018



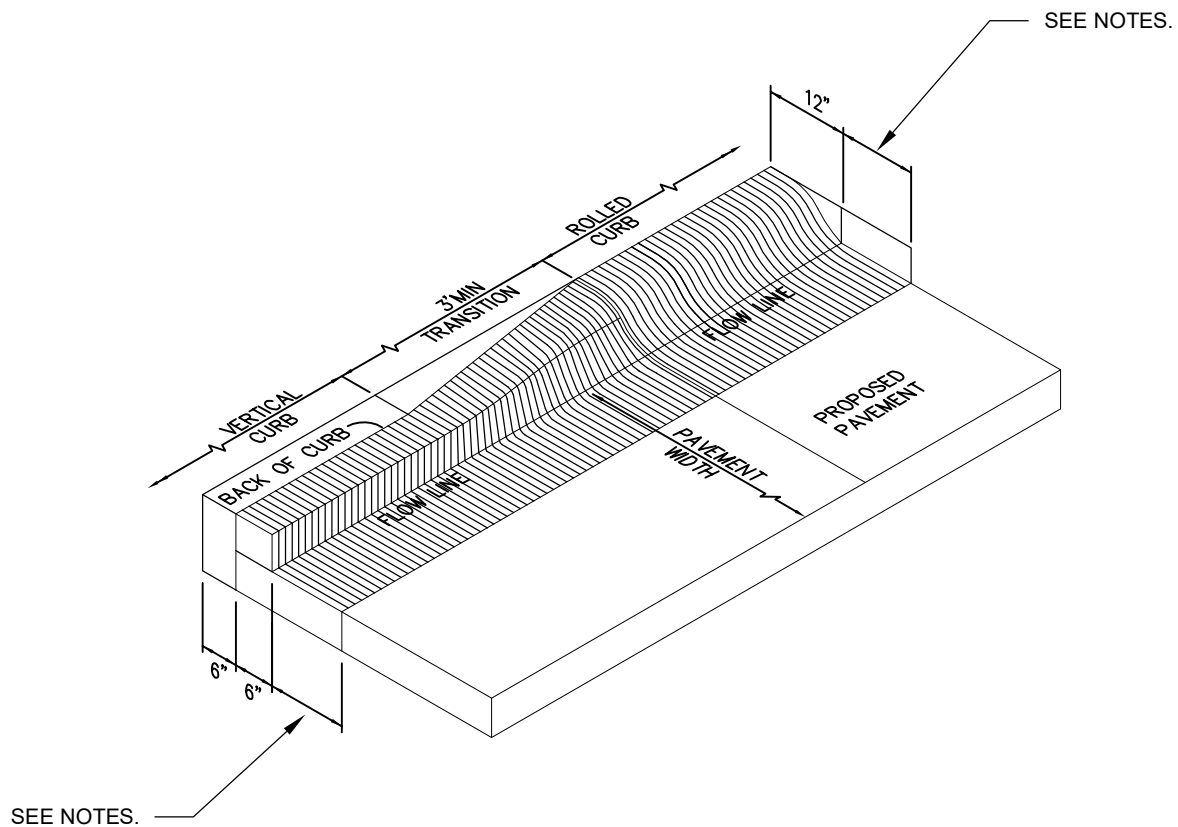
City of Redmond
WASHINGTON

STANDARD DETAILS

ROLLED CURB AND
GUTTER SECTION

FILE NAME: SD322.DWG

DETAIL NUMBER: 322



CURB TRANSITION

NTS

NOTES:

1. 18 INCH, BASED ON TYPE "A-1" IN TYPICAL CURB AND GUTTER DETAIL SD 304.
2. 12 INCH MAY BE PERMITTED FOR NEW PRIVATE STREET OR MATCHING EXISTING. SEE NOTE 2 AND 3 IN SD 304

Rob Crittenden

APPROVED BY: ROB CRITTENDEN
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: MARCH 01, 2018



City of Redmond
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STANDARD DETAILS

CURB TRANSITION

FILE NAME: SD323.DWG

DETAIL NUMBER: **323**